

**Tuesday June 26, 2018
Medway Planning and Economic Development Board
155 Village Street
Medway, MA 02053**

Members	Andy Rodenhiser	Bob Tucker	Tom Gay	Matt Hayes	Rich Di Iulio
Attendance	X	Remote Participation	X	X	X

ALSO PRESENT:

- Amy Sutherland, Recording Secretary
- Susy Affleck-Childs, Planning and Economic Development Coordinator

The Chairman opened the meeting at 7:00 pm.

There were no Citizen Comments.

Consultant Bouley was contacted by telephone and participated remotely.

PEDB Meeting Minutes:

June 12, 2018:

On a motion made by Rich Di Iulio, and seconded by Matt Hayes, the Board voted unanimously to accept the minutes from the June 12, 2018 meeting.

Town Line Estates – Subdivision:

The Board is in receipt of the following document: **(See Attached)**

- Email from Attorney Paul Kenney dated June 19, 2018.

The members were informed that Paul Kenney is requesting a 60 day extension for the Board's endorsement of the Town Line Estate Definitive Subdivision Plan. This is because the subject property is registered land and any plans have to be approved by the Land Court.

On a motion made by Rich Di Iulio and seconded by Matt Hayes, the Board voted unanimously to approve a 60 day extension for Town Line Estates.

Millstone ARCPUD

The Board was made aware that the Board of Selectmen at their last meeting approved an adjustment in the pricing of the affordable dwelling units at Millstone. The Conservation Commission has agreed to accept an easement for the proposed Millstone Trail. The developer requests from the PEDB to relocate the public parking area for the Millstone Open Space from Lovering Street to the east side of Cobblestone Drive. A drawing of this was shown **(See Attached)** and reviewed by the Board and it is the opinion that this is a safer location. The

Board does not think this is a major modification and does not warrant a public hearing or notice. There will be an addendum to the decision filed with the Town Clerk.

On a motion made by Rich Di Iulio and seconded by Matt Hayes, the Board voted unanimously to approve the change in the public parking for the Millstone ARCPUD open space area to the east side of Cobblestone Drive.

Salmon Willows ARCPUD Minor Modification:

The Board is in receipt of the following document: **(See Attached)**

- June 21, 2018 letter from David Harrington of Coneco Engineering

Jeff Robinson of Continuing Care Management attended the meeting. He is proposing a change in treatment of the small parking lot near the gazebo at the head of the trail from pervious pavers to pervious pavement. Consultant Bouley indicated that he has no issue with the pervious paving in the parking area. He recommended that the operation and maintenance plan be updated to show this. It was suggested that there be signage to direct maintenance personnel to avoid using sand as it clogs up pervious pavement. The applicant agreed to this. Conservation Agent Bridget Graziano has reviewed this and provided an email dated June 25, 2018 that there needs to be an amendment to the LTPP (Long Term Pollution Prevention Plan).

On a motion made by Matt Hayes and seconded by Tom Gay, the Board voted to approve the minor modification for Salmon Willows to allow the changing of pervious pavers to pervious paving in the parking area near the gazebo. (Member Di Iulio abstained)

NOTE - Member Tucker joined the meeting by telephone at 7:20 pm. **(See Attached Remote Participation Form)**

Public Hearing Continuation - Converting Technical Services Site Plan, 9 Trotter Dr.:

The Board is in receipt of the following documents: **(See Attached)**

- Public Hearing Continuation Notice filed with the Town Clerk on 6-20-18
- Response letter dated 6-12-18 from Engineering Design Consultants, Inc. in response to review letters from Tetra Tech and PGC Associates
- Revised site plan dated 6-12-18
- Trailer turning exhibit dated 6-12-18.
- Revised building elevation and floor plan dated June 20, 2018.
- Requests for waivers from *Site Plan Rules and Regulations*

Member Hayes informed the Board that he has reviewed the recording of the June 12 hearing and will submit the paperwork for the Mullin's Rule certification.

Project Engineer, Peter Bemis from CTS informed the Board that he met with the DRC to review and discuss the revised façade elevations. There will be a letter from DRC forthcoming.

Mr. Bemis discussed the review letter from Tetra Tech and noted the following revisions which they have made to the plan.

- Assessor's info regarding zoning district added.
- Utilities have been added
- Dimensions have been added to layout plan.
- Horizontal sight distances have been added.
- A parking schedule has been added to the plan.
- Turning move and template has been added for review.
- Sidewalk in front of the building has been widened to 7 feet for the vehicles' overhang and the curb stops have been removed.
- The cut-off shields for the light fixtures were modified so there is no longer light trespass. (Consultant Carlucci responded that this needs to be further revised to meet the standard for no light trespass onto Trotter Drive.)
- Test pit data is now on plan
- Proposing Cape Cod berm for the entrances.
- Layout eliminates one parking space in the southeast corner of the site.
- The property owner would like the sewer easement prepared for the sewer connection through the easement to the first sewer manhole.

Mr. Bemis explained that there was an alternative for the sewer easement going south and bringing it diagonally from the manhole in Trotter Drive to the property. The Chairman prefers the line to go right in straight and does not want the diagonal alternative. The applicant expresses there is a cost analysis which he is faced with. The PEDB wants to see what the applicant comes up with after this is budgeted out. The easement would have to allow the town to assess the sewer line if needed. Susy Affleck-Childs reported that there have been no review comments from DPS. Mr. Bemis will reach out to DPS.

The various waiver requests were reviewed:

- 204-4: Site Plan Preparation 1' = 40' : Board ok with waiver
- 204-5: Site Plan Content: Existing Landscape Inventory: The applicant showed the no cut line. Applicant will get the exact locations. Board ok with waiver.
- 205-6 Parking G. Parking Spaces/Stalls – Parking stalls shall not be located within 15 ft. of the front, side and rear property lines. Board ok with waiver.
- Stalls shall be 10' x 20' creating one way traffic flow parking spaces are sufficiently sized. Board ok with waiver. (Will fix regulations in future)
- 205-6: Wheel stops. Board ok with waiver due to wider sidewalks.
- 204-3 Planning Board submittal – The project scale and scope is small being only 6,000 s.f.
- 205.5-Site Plan Contents (c.3. existing landscape inventory) – the site is partially wooded. Board ok with waiver from having a certified landscape architect prepare a landscape inventory
- The Board would like the roundings at Trotter Drive to be granite and then vertical Cape Cod berm elsewhere is fine.

On a motion made by Rich Di Iulio and seconded by Matt Hayes, the Board voted unanimously by roll call vote to continue the hearing to July 10, 2018 at 7:05 pm.

Roll Call:

Rich Di Iulio	aye
Andy Rodenhiser	aye
Bob Tucker	aye
Tom Gay	aye
Matt Hayes	aye

Susy will begin drafting the decision for the Board's review at the next meeting.

CONSTRUCTION REPORTS:

The Board is in receipt of the following construction reports: **(See Attached)**

- Emails dated June 6, 15 and 22 from Steve Bouley re: Applegate Subdivision.
- Beals and Thomas report dated June 15, 2018 for Exelon project.

Chairman Rodenhiser stepped away during discussions regarding Applegate.

The Board was made aware that a site visit was done at Applegate on June 6, 2018. The Conservation Agent has also been to the site and provided an email dated June 15, 2018. There was roadway sediment runoff from various construction lots. There should be roadway sweeping which is maintained on a regular basis. There is runoff which is ponding since the silt sacks have sediment in them. There has also been a tree which has fallen across the basin outlet structure. This needs to be removed along with the woody vegetation which is growing in the rip rap.

Mr. Costello has not responded to any of the communications.

The Board would like a certified letter sent to Mr. Costello along with notification to the bank. The Board would like this placed on the July 10, 2018 agenda.

The Chairman rejoined the discussion.

Member Gay excused himself from discussion of Exelon.

The construction report from BETA indicated that Exelon is doing the installation of earthen containment berm around tank. The installation of the piping is in progress.

Member Gay rejoined the discussion.

Consultant Bouley concluded his telephone participation in the meeting at 8:15 pm.

REORGANIZATION:

Chairman:

On a motion made by Matt Hayes and Tom Gay, the Board voted unanimously by roll call vote to appoint Andy Rodenhiser as Chairman of the Planning and Economic Development Board.

Roll Call Vote:

Matt Hayes	aye
Tom Gay	aye
Bob Tucker	aye
Rich Di Iulio	aye
Andy Rodenhiser	aye

Vice Chairman:

On a motion made by Matt Hayes and Andy Rodenhiser, the Board voted unanimously by roll call vote to appoint Bob Tucker as Vice Chairman to the Planning and Economic Development Board.

Roll Call Vote:

Matt Hayes	aye
Tom Gay	aye
Bob Tucker	aye
Rich Di Iulio	aye
Andy Rodenhiser	aye

Clerk:

On a motion made by Matt Hayes and Rich Di Iulio, the Board voted unanimously by roll call vote to appoint Tom Gay as Clerk to the Planning and Economic Development Board.

Roll Call Vote:

Matt Hayes	aye
Tom Gay	aye
Bob Tucker	aye
Rich Di Iulio	aye
Andy Rodenhiser	aye

The members will be keeping the various liaison appointments from the previous year. **(See Attached.)** The Board will wait to appoint a member to the EPFRAC committee until after the BOS decides whether it wants to have a PEDB member on EPFRAC.

Member Tucker concluded his remote telephone participation in the meeting at 8:32 pm.

The Haven Subdivision:

The Board is in receipt of the paperwork for the lot release of Sorrento Lane. **(See Attached)** The developer expects to convey the first house in July. NOTE – The Board signed the lot release.

Zoning Work for Fall Town Meeting:

The Board reviewed the handout provided by Susy Affleck-Childs. This is a list of staff ideas for zoning work for the fall town meeting. **(See Attached)** The Board discussed working on modifications to the multifamily overlay district provisions to protect historic resources, clarify exceptions, adjust minimum frontage and strengthen buffer language. It was also suggested to

add Museum to the table of uses. There was also discussion from the Oak Grove Zoning Task Force about putting a moratorium in place to freeze development in this area while the new zoning is developed.

CORRESPONDENCE: (See Attached)

- Flyer re: June 25 OSRP Community Forum
- 495 MetroWest Partnership Annual Report
- Demographic data report from 495 MetroWest Partnership

OTHER BUSINESS:

- Member Hayes informed all that the Open Space and Recreation Plan Task Force held was Community Forum in regards to the Open Space & Recreation Plan. This was a well-attended meeting which discussed thoughts and ideas about goals and priorities for Medway's new Open Space and Recreation Plan. It is still in draft form. All the comments from the night will be incorporated into the plan.
- The Board would like Susy to follow-up with Jack Mee about renovations which are going in at Summer Street (Berry's Greenhouse).
- 57 Main Street is doing work and has not applied for site plan review. Susy will follow-up with Jack Mee.

FUTURE MEETING:

- Tuesday, July 10, 2018.

ADJOURN:

On a motion made by Tom Gay and seconded by Matt Hayes, the Board voted unanimously to adjourn the meeting.

The meeting was adjourned at 9:01 pm.

Prepared by,



Amy Sutherland
Recording Secretary

Reviewed and edited by,



Susan E. Affleck-Childs
Planning and Economic Development Coordinator



June 26, 2018
Medway Planning & Economic Development Board
Meeting

Town Line Estate Subdivision

- Email from Attorney Paul Kenney dated June 19, 2018 regarding a delay in plan endorsement due to required Land Court filing

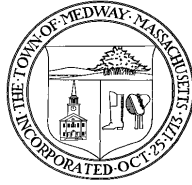
Susan Affleck-Childs

From: Paul Kenney <pkenney@kenney-law.com>
Sent: Tuesday, June 19, 2018 1:44 PM
To: Susan Affleck-Childs
Cc: rlapinsky@gmail.com; lwasnewski@gmail.com; jeff.kane1@verizon.net; survey@clsurveyma.com
Subject: Town Line Estate Subdivision

Susy,
As a follow up to our conversation, please accept this email as a request for a 60 day extension for the approval of the Town Line Estate subdivision. As reason for the request, the property is registered land, and in connection with any subdivision of registered land, the Land Court Engineering office in Boston requires a pre-filing review of the plan with the surveyor's computations, including line closures, perimeter closure, street and lot closures, as well as attested copies of the current Certificate of Title. I am forwarding the required documents to the Land Court, however, the review may take some time to be completed by the engineering office. Once approved by the Land Court engineering office, the final plan signed by the Planning Board can be submitted for approval. If there are any questions, please let me know. Thank you.

Paul

Paul V. Kenney
Kenney & Kenney
181 Village Street
Medway, MA.
02053
Phone: 508-533-6711
Fax: 508-533-6904



June 26, 2018
Medway Planning & Economic Development Board
Meeting

Millstone ARCPUD – Change in Parking
for Open Space

At its June 14th meeting, the Conservation Commission agreed to accept an easement for the proposed Millstone Trail. At its June 18th meeting, the BOS approved an adjustment in the pricing of the affordable dwelling units at Millstone.

The PEDB is now able to consider the developer's request to relocate the public parking area for the Millstone open space from Lovering Street to the east side of Cobblestone Drive. See attached plan last revised June 7, 2018 by GLM showing the changed parking location and the trail easement route.

I would ask the Board to take a straw poll vote to ascertain your perspective on this change. CED Director Barbara Saint Andre and I feel this proposal merits a formal application to modify the Millstone ARCPUD special permit plan (with public notice and a hearing) as this pertains to a change in the public's access to the open space which is an essential component of the approved Millstone development and plan. Please note that the hearing would pertain only to the parking area, not to the planned trail on the Millstone property.

Mr. Venincasa would much prefer that the Board not require the full public hearing process. He does not plan to attend Tuesday night's meeting.



June 26, 2018
Medway Planning & Economic Development Board
Meeting

Salmon Willows ARCPUD –
Minor Modification

- June 21, 2018 letter from David Harrington, PE at Coneco Engineering re: changing pervious pavers to pervious paving in parking area around Gazebo and trailhead.



ENVIRONMENTAL
ECOLOGICAL
ENERGY
SURVEY
CIVIL

June 21, 2018

Town of Medway
Planning & Economic Development Board
Attn.: Mr. Andy Rodenhiser & Members of the Planning Board
155 Village Street
Medway, Massachusetts 02053

**Subject: Salmon Health and Retirement Community – Village Street
Changes to Accessory Parking Lot
259, 261, 261R and 263 Village Street, Medway, Massachusetts**

Dear Mr. Andy Rodenhiser & Members of the Planning Board:

On behalf of Continuing Care Management, LLC, Coneco Engineers & Scientists, Inc. (Coneco) is pleased to submit the enclosed supporting documentation for Salmon Health and Retirement Community. These documents address plan changes to the 3,554 square foot accessory parking lot near the northwest entrance to the site.

The previously approved design proposes the accessory parking lot to be constructed of permeable pavers spaced with aggregate. The proposed change entails using a layer of porous asphalt instead (as shown in the attached detail). The change in permeable pavement type will only impact construction methods and appearance as both are options for porous pavement in the Structural BMP section of the MassDEP Stormwater Handbook. This modification should have no effect on the performance or functionality of the parking lot. Additionally, it should be noted that there will be no changes to the drainage systems or stormwater runoff from these design changes.

Please contact me at 508-697-3191 ext. 135 with any questions. Thank you for your time and consideration in this matter.

Sincerely,

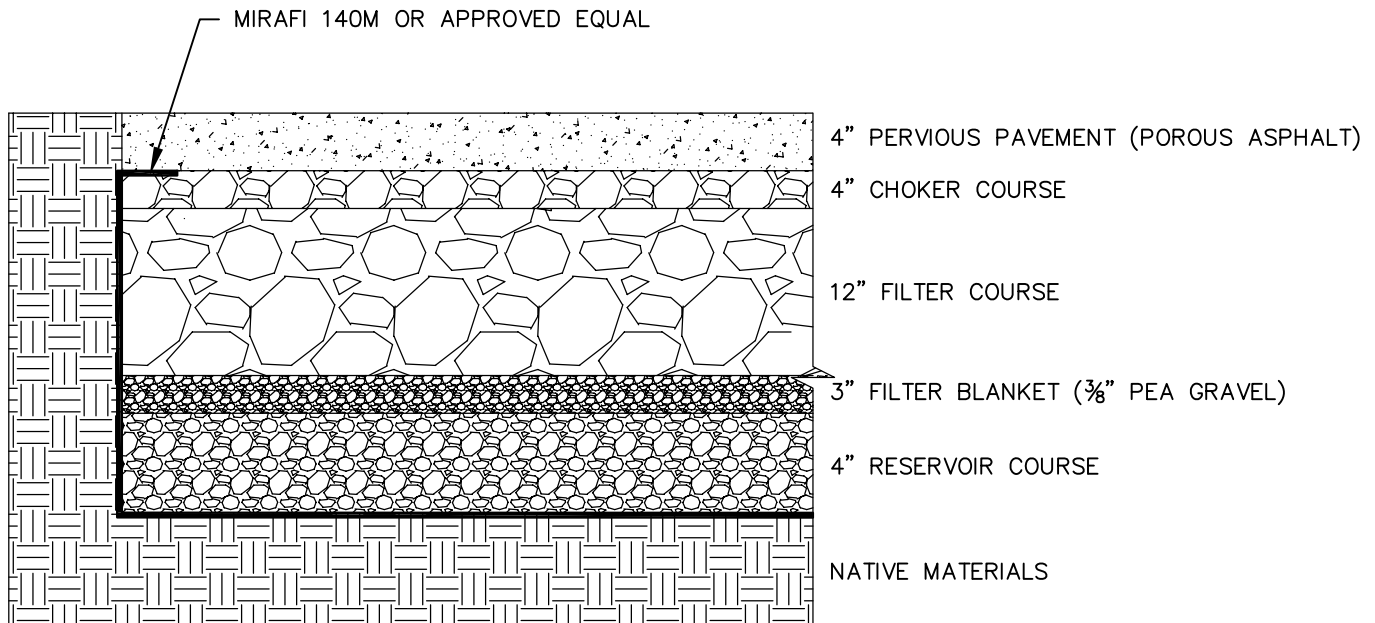
A handwritten signature in blue ink, appearing to read 'D. Harrington'.

David Harrington, P.E.
Senior Civil Engineer

US STANDARD SIEVE SIZE INCHES/MM	PERCENT PASSING (%)			
	CHOKER COURSE (AASHTO NO. 57)	FILTER COURSE (MODIFIED NHDOT 304.1)	RESERVOIR COURSE (AASHTO NO. 3)	RESERVOIR COURSE ALTERNATIVE* (AASHTO NO. 5)
6/150	—	100	—	—
2½/63	—	—	100	—
2/50	—	—	90 — 100	—
1½/37.5	100	—	35 — 70	100
1/25	95 — 100	—	0 — 15	90 — 100
¾/19	—	—	—	20 — 55
½/12.5	25 — 60	—	0 — 5	0 — 10
⅜/9.5	—	—	—	0 — 5
#4/4.75	0 — 10	70 — 100	—	—
#8/2.36	0 — 5	—	—	—
#200/0.075	—	0 — 6**	—	—
% COMPACTION ASTM D698/ AASHTO T99	95	95	95	95

* ALTERNATE GRADATIONS (E.G. AASHTO NO. 5) MAY BE ACCEPTED UPON ENGINEER'S APPROVAL.

** PREFERABLY LESS THAN 4% FINES.



259, 261, 261R AND 263 VILLAGE STREET, MEDWAY, MASSACHUSETTS 02053



PREPARED FOR:
CONTINUING CARE MANAGEMENT, LLC

PLAN SET:
CONSTRUCTION DOCUMENTS

NOT TO
SCALE

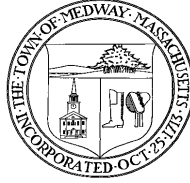
DATE
06/21/2018

PROJECT NO.
8548.0

TITLE:
POROUS ASPHALT
PAVEMENT DETAIL



6/22/2018



June 26, 2018

**Medway Planning & Economic Development Board
Meeting**

**Converting Technical Services Site Plan
– Public Hearing Continuation**

UPDATED – June 25, 2018

- Public hearing continuation notice filed with the Town Clerk on 6-20-18
- **Project engineer's response letter dated 6-12-18 to PEDB consultants' comments**
- **Revised site plan dated 6-12-18**
- **Trailer turning exhibit dated 6-12-18**
- **Revised building elevation and floor plan dated 6-20-18**
- Requests for waivers of the *Site Plan Rules & Regulations*

CTS project engineer Peter Bemis met with the DRC on Monday, June 18 and showed them revised façade elevations based on discussion at the first PEDB public hearing. A review letter from the DRC is forthcoming.

I have also attached the requests for waivers from the *Site Plan Rules and Regulations*. I would like you to review and discuss them at the meeting so I can get a sense of which ones you are OK with.

My plan is to have a decision for you to consider at the July 10th meeting.




TOWN OF MEDWAY
Planning & Economic Development Board
155 Village Street
Medway, Massachusetts 02053

Andy Rodenhiser, Chairman
Robert K. Tucker, Vice-Chairman
Thomas A. Gay, Clerk
Matthew Hayes, P.E.
Richard Di Iulio

MEMORANDUM

June 18, 2018

TO: Maryjane White, Town Clerk
Town of Medway Departments, Boards and Committees

FROM: Susy Affleck-Childs, Planning & Economic Development Coordinator 

RE: **Public Hearing Continuation: Converting Technical Services, 9 Trotter Drive**
Continuation Date: Tuesday, June 26, 2018 at 7:05 p.m.
Hearing Location: Medway Town Hall – Sanford Hall, 155 Village Street

At its meeting on June 12, 2018, the Planning and Economic Development Board voted to continue the public hearing on the application of CTS Property Management of Franklin, MA for approval of a major site plan for the proposed construction of a light manufacturing facility at 9 Trotter Drive. **The continued public hearing will take place on Tuesday, June 26, 2018 at 7:05 p.m. in Sanford Hall at Medway Town Hall, 155 Village Street.**

The subject property is 1.14 acres and is located on the east side of Trotter Drive in the 495 Business Park in the West Industrial zoning district. The parcel, shown on the Medway Assessors Map as #54-004-0004, is owned by Marguerite Mele of Medway, MA.

The proposed project includes construction of a 6,000 sq. ft. one-story building with associated parking and loading facilities for Converting Technical Services, a light manufacturing/warehouse business. The site plan shows parking for 21 vehicles, landscaping, stormwater drainage facilities, site and building lighting, and connections to municipal water and sewer. Site access/egress is planned with two curb cuts on Trotter Drive.

The plan is titled *9 Trotter Drive Site Plan*, is dated March 26, 2018 and was prepared by Engineering Design Consultants, Inc. of Southborough, MA.

The application, site plan, and other documents are on file with the Medway Town Clerk and at the office of the Planning and Economic Development Board at Medway Town Hall, 155 Village Street and may be reviewed during regular business hours. The application, site plan, and other documents are also posted at the Planning and Economic Development Board's web page at: <https://www.townofmedway.org/planning-economic-development-board/pages/9-trotter-drive-converting-technical-services-major-site>

Please contact me if you have any questions. Thanks.

Telephone: 508-533-3291 Fax: 508-321-4987
planningboard@townofmedway.org

June 12, 2018

Planning Board
Town Hall
Medway, Massachusetts 02053

Reference:

9 Trotter Drive
Medway, Massachusetts
EDC Job No.: 3583

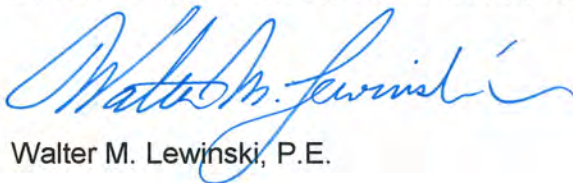
Dear Board Members:

In reference to the above project, the following is a response to peer review comments provided by Tetra Tech, in a letter dated June 6, 2018 and a letter also dated June 6, 2018 from PGC Associates LLC.

The response to comments are provided in red.

Very truly yours,

ENGINEERING DESIGN CONSULTANTS, INC.



Walter M. Lewinski, P.E.

Tetra Tech review comments:

- 1) The applicant has not supplied a written Development Impact Statement. A waiver has been requested from this Regulation. (Ch. 200 §204-3.A.7) **Waiver pending with Board, no response necessary.**
- 2) The Plans are drawn at a scale of 1"=20', Regulations require 1"=40'. However, the plans as drawn are sufficient to adequately show the proposed work. A waiver has been requested from this Regulation. Waivers have been granted from this Regulation on previously approved projects. (Ch. 200 §204-4.B) **Waiver pending with Board, no response necessary.**
- 3) Project assessors map and parcel number and zoning district classification are not shown on the cover sheet. (Ch. 200 §204-5.A) **The Town of Medway Assessors' ID as well as the zoning district have been added to the cover sheet.**
- 4) Existing utilities have not been shown on the Existing Conditions Plan. (Ch. 200 §204-5.C.1) **The existing utilities have been added to the plans.**
- 5) The applicant has not supplied an Existing Landscape Inventory. A waiver has been requested from this Regulation. (Ch. 200 §204-5.C.3) **Waiver pending with Board, no response necessary.**
- 6) Setbacks from property lines to proposed parking limits and curb radii have not been included on the Plans. (Ch. 200 §204-5.D.2) **The dimensions have been added to the proposed layout plan.**
- 7) The applicant has not included dimensioning of the proposed north exit driveway, loading dock and loading maneuvering area. (Ch. 200 §204-5.D.3) **The dimensions have been added to the proposed layout plan.**
- 8) Proposed landscape plan does not appear to be designed by a registered landscape architect and does not include existing landscape inventory, particularly along the property boundaries. There are notes (Spirea and Daylily) included in the plan that are not pointing to the correct location. (Ch. 200 §204-5.D.7) **Waiver on inventory pending with Board, however landscape design edits as requested by staff and Design Review Committee have been completed.**
- 9) Color Renderings of the site have not been submitted. (Ch. 200 §204-5.D.9) **Waiver pending with Board, no response necessary.**
- 10) Horizontal sight distances are not shown on the Plan. (Ch. 200 §204-5.D.14) **Horizontal sight distances have been added to the plans.**
- 11) The applicant has not supplied parking information on the zoning table. (Ch. 200 §204-5.D.15) **A parking schedule has been added to the layout plan.**
- 12) Existing or proposed hydrants have not been included on the Plans. Truck turning template should be run on the site to determine if fire apparatus can maneuver sufficiently. Confirmation from Medway Fire Chief recommended. (Ch. 200 §204-5.D.16) **We will supply a turning movement template analysis for your review.**
- 13) The applicant is proposing two curb cuts to access the site. Regulations recommend one per street frontage. (Ch. 200 §205-3.A.2.a) **The limitations with the depth of the property and sloping land on the northerly side of the property require the design to incorporate angled parking and an exit only for the front portion of the parking lot.**
- 14) The applicant is proposing cape cod berm in lieu of vertical granite curbing, which does not comply with the Regulations. (Ch. 200 §205-3.B.6) **Waiver pending with Board, no response necessary.**

- 15) Designated employee parking areas have not been shown on the Plans. (Ch. 200 §205-6.C) **Waiver pending with Board, no response necessary.**
- 16) The applicant is proposing 9' x 18' standard parking stalls which do not comply with the Regulations. A waiver has been requested from this Regulation. (Ch. 200 §205-6.G.3.a) **Waiver pending with Board, no response necessary.**
- 17) Wheel stops are required in parking stalls abutting sidewalks. However, we recommend extending the depth of the parking stalls and eliminating wheel stops as they tend to cause issues during winter months during snow plowing operations. A waiver has been requested from this Regulation. (Ch. 200 §205-6.G.3.b) **The sidewalk has been widened to seven feet for the vehicles overhang.**
- 18) Parking stalls are located within the fifteen-foot minimum parking setback. Parking shall not be located within 15-feet of front-side or rear property lines. A waiver has been requested from this Regulation. (Ch. 200 §205-6.G.4.b) **Waiver pending with Board, no response necessary.**
- 19) The final space located along the southern parking area does not include the required "maneuvering strip". (Ch. 200 §205-6.G.4.d) **Last parking space eliminated in order to properly address this provision.**
- 20) The applicant has not shown proposed snow removal areas on the Plans. A note exists however it should be graphically shown in areas where the snow will be placed. (Ch. 200 §205-7) **Snow storage areas have been labeled on the layout plan (sheet 3).**
- 21) Proposed foot-candle readings exceed the minimum allowed by the Regulation at the property lines. Light spill onto neighboring properties should not occur at the site from proposed lighting. (Ch. 200 §205-8) **The cut-off shields were further adjusted by the lighting design company and there is no longer light trespass.**
- 22) The applicant has not provided existing tree inventory of the site and thus cannot determine if tree replacement is necessary or how many trees will be required to be replaced. (Ch. 200 §205-9.F) **Waiver pending with Board, no response necessary.**

The following items were found to not be in conformance with MA DEP Storm Water Management Standards and/or Town stormwater standards.

MA DEP Stormwater Management Standards

- 23) Proposed runoff in the 2-year event for the "West" design point is greater than existing. Post development peak runoff for all storms required must be less than pre-development conditions. (Standard 2) **The slight increase in the peak developed runoff for the 2-year event is due to the computer program roundoff and the 0.1 cfs increase is insignificant, if we rounded the results to the tenth, the results for the predeveloped and post-developed runoff would be zero. The volume of runoff is less for the post-developed state also, 219 cubic feet for post-developed versus 240 cf for the existing.**
- 24) The applicant has not provided test pit information for the Site. Test pit information is required to confirm hydrologic soil groups and ground water elevation at the proposed recharge locations. (Standard 3) **Test pit data was inadvertently set to a non-plotting layer and that has now been corrected and is on the site plans.**

PEDB Stormwater Regulations (Ch. 200 §205-4)

- 25) The applicant has not provided test pit information for the proposed recharge areas. (Ch. 200 §205-4.B) **Test pit data was inadvertently set to a non-plotting layer and that has now been corrected and is on the site plans.**

Town Stormwater Bylaw (Article XXVI)

- 26) We recommend the applicant provide a narrative documenting compliance with "Article XXVI Stormwater Management and Land Disturbance" of the Town of Medway General Bylaws. A SWPPP Plan has been made part of the Site Plan set, however the site alters less than 1 acre of land and will not required a NPDES Permit from EPA.

General Stormwater Comments

- 27) Two types of subsurface infiltration BMPs are proposed for the Project, a Cultec Recharger system and Precast Concrete Flow Diffuser system. We recommend the applicant provide commentary on the decision to place two different systems in the design. The Cultec chambers are located in a grassed area and the concrete flow diffusers are located under pavement to stand up to the truck loads.
- 28) We recommend the applicant provide greater detail on the Concrete Flow Diffuser system proposed for stormwater management of the parking area. It is unclear if the diffusers are placed on crushed stone, wrapped with fabric or if an "Isolator Row" or "Separator Row" type of TSS removal treatment can be incorporated into the design. We also recommend adding manhole at each inlet point for ease of inspection and maintenance. Inspections ports will also be required throughout the limits of the BMP. We have added a note to the detail indicating that the flow diffusers shall be placed on 6" of clean crushed stone and the entire system shall be surrounded by 1-foot of clean crushed stone and a 4 oz. non-woven filter fabric. The design does include an inspection manhole above the inlet pipe and outlet pipe, more inspection ports are not required because the VortSentry's will provide the required TSS removal.
- 29) We recommend the applicant provide O&M Plan and appurtenant inspection/maintenance log forms in the Stormwater Report to properly direct post-construction maintenance personnel on inspecting and maintaining the stormwater system. Maintenance requirements should be clearly stated on the forms. We have prepared a Bi-Annual Stormwater Inspection Report for the project.

The following is a list of general items that TT recommends the applicant take into consideration prior to the next submission:

- 30) We recommend the applicant provide written confirmation from abutting property owner of proposed sewer easement. Furthermore, it is unclear why the easement is necessary as a connection could be made directly from SMH #1 to the existing manhole located in Trotter Drive. If it is to limit disturbance to the right-of-way, a perpendicular connection should be made from SMH #2 to the existing manhole. The property owner had preferred that we prepare a sewer connection through the easement to the first sewer manhole as drawn.
- 31) We recommend the applicant label proposed vertical granite curb at both curb cuts on the Plan. The existing curb lines along Trotter drive are bituminous concrete which is why we have proposed cape cod berm for the entrances.

PGC LLC review comments:

Zoning

1. The proposed primary uses are for light manufacturing and warehouse. This is allowed in the West Industrial zoning district, and the proposed development appears to comply with the Zoning Bylaw. **No response necessary.**
2. The plan proposes 20 parking spaces, including 1 van-accessible handicapped space, and 19 standard parking spaces. No parking calculation was provided. The Zoning Bylaw requires 1 space per 300 square feet of office space and 1 space per 2 employees and 1 per 1000 square feet of manufacturing or warehouse space. The proposed building has 264 square feet of office space and the maximum number of employees expected is 10. Therefore 1 space is required for the office space, 5 for the ten employees and 6 for the square footage of the building for a total of 12 spaces required. **The lighting schedule has been added to the layout plan, a total of 20 parking spaces are provided including one "Van Accessible" space.**
3. Section 7.1.2 states that light trespass onto any abutting street or lot is not permitted. There is light trespass ranging from .1 to .8 foot-candles on 3 sides of the lot. **The cut-off shields were further adjusted by the lighting design company and there is no longer light trespass.**
4. The dimensional requirements of Table 2 of Section 6 limit impervious coverage to 80% of the lot and requires 20% open space. The plan documents that it meets these requirements with 52% open space and 48% impervious surface. **No response necessary.**
5. The plans show a location for a monument sign. The proposed dimensions of 20 feet per side comply with the maximum size requirements of 40 square feet total as well as the required setback of 10 feet. No height off the ground is shown but the maximum height is 6 feet. No landscaping is shown at the base of the sign (and the sign is not shown on the landscape plan). **The monument sign has been added to the landscape plan with plantings.**
The building elevations do not show a building sign. **A building mounted sign is an option but may be an expense that the owner cannot afford at this time.**
6. Section 7.1.1 G requires parking not be located in front of the building unless no reasonable alternative exists. **The site is designed to provide the necessary truck turning movements and to have accessible access to the building, with significant elevation rise on the northern portion of the parcel, no reasonable alternative exists.**
7. Under Section 7.1.1 I, a bicycle parking spot is required unless the PEDB waives this requirement. The waiver is requested. **Waiver pending with the Board, no response necessary.**

Site Plan Rules and Regulations

8. Section 204.3 A. (7) requires a Development Impact Report. Most elements are only applicable to projects with 30 or more parking spaces so mostly not applicable to this project. A waiver is requested but a letter describing minimal traffic generation is provided. **Waiver pending with the Board, no response necessary.**
9. Section 204-5 B. requires a Site Context Sheet. This was not provided. **Cover sheet includes locus map item 1 and existing conditions sheet has abutters, metes & bounds, topography, and zoning.**
10. Section 204-5 C. (3). The Existing Conditions Sheet also does not include an Existing Landscape Inventory prepared by a Landscape Architect. A waiver is requested based on the lack of significant landscape features on the site. **Waiver pending with the Board, no response necessary.**
11. Section 204-5 D. (3) requires dimensions of proposed improvements. This was not provided for some features, including the northerly entrance drive, loading zone area, maneuvering area and other features. **The dimensions have been added to the layout plan.**
12. Section 204-5 D. (7) requires that a landscape architect prepare the landscape plan. A landscape plan was provided but there is no indication it was prepared by a landscape architect. **Waiver pending with the Board, no response necessary.**
13. Section 204-5 D. (12) requires a signage plan indicating the design, location, materials, dimensions and lighting. As stated above, a monument sign location and dimensions are indicated on the plan, but no other details are provided. There is no indication of a building sign. **A monument sign is proposed and a building mounted sign remains an option is the owner can manage that cost within his project budget.**
14. Section 204-5 D. (13) requires a lighting plan. As stated above, no lighting plan has been provided, but it shows light trespass onto abutting properties. **A Landscape & Lighting Plan is included within the plan set.**
15. Section 204-5 (14) requires horizontal sight distances be show on the plan. This information was not provided. **The sight distances have been added to the plans.**
16. Section 204-5 (16) requires information about fire prevention and suppression. This was not provided. **The proposed building is sized below the threshold mandating fire suppression.**
17. Section 205-3 D requires pedestrian-friendly sidewalks in front of site. This is not provided and no waiver is requested **A sidewalk is provided along the front of the building and there is an existing sidewalk along the west sideline of Trotter Drive.**

18. Section 205-6 G. (3)(b) requires wheel stops where a parking space abuts a walkway. A waiver is requested to allow a curb instead. **The sidewalk has been widened in order to avoid installing curb-stops.**
19. Section 205-6 G. (4)(d) requires a 12' x 20' turning area at the end of the parking row. This is not provided and no waiver is requested. **Last parking space eliminated in order to properly address this provision.**
20. Section 205-6 (H) requires vertical granite curbing "or similar" around the perimeter of a parking lot. The plan proposes Cape Cod berm. **Waiver pending with the Board, no response necessary.**
21. Section 205-9 C requires that there be substantial landscaped islands within parking lots to reduce the "sea of asphalt" effect. More specifically, Section 209-6 C requires at least 1 deciduous tree per 6 spaces and only trees that provide shade to the parking area are to count toward this requirement. With 20 spaces, 4 trees are required. The trees proposed along the front of the property likely meet this requirement (at least when grown), but consideration should be given to adding trees in the islands at the ends of the parking rows. **We have provided landscape islands at the end of a row of 11 spaces and 9 spaces which is not a vast amount of asphalt without adjacent greenspace and trees are proposed along Trotter Drive and at the loading dock to more than exceed the minimum requirement of 1 per 6 spaces.**

General Comments

22. The plan appears to meet the criteria specified in Section 203-9 C. **No response necessary.**

Bi-Annual Stormwater Inspection Report

General Information

Project Name : 9 Trotter Drive	
Location : Medway, MA	
Date of Inspection:	Start/End Time:
Inspector's Name:	
Inspector's Title:	
Inspector's Contact Information:	
Describe present site conditions :	
Type of Inspection : <input type="radio"/> Regular (Bi-Annual) <input type="checkbox"/> Pre-storm event <input type="checkbox"/> During storm event <input type="checkbox"/> Post-storm event	

Onsite Storm Water Systems

Are roof drains present and operational? <input type="checkbox"/> Yes <input type="checkbox"/> No If no provide details:
Do VortSentry structures need cleaning? (2-feet of sediment, see VortSentry HS Maintenance guide) <input type="checkbox"/> Yes <input type="checkbox"/> No If yes, notify stormwater system owner for cleaning.
Is evidence of subsurface leaching area operational? <input type="checkbox"/> Yes <input type="checkbox"/> No Ifno Provide details:
Are erodible surfaces present since the last inspection? <input type="checkbox"/> Yes <input type="checkbox"/> No If yes, describe:
Are there any illicit discharges at the time of inspection? <input type="checkbox"/> Yes <input type="checkbox"/> No If yes, describe:

Certification Statement

"I certify that this document and all attachments were prepared under my direction or supervision in accordance with a system designed to assure that qualified personnel properly gathered and evaluated the information submitted. Based on my inquiry of the person or persons who manage the system, or those persons directly responsible for gathering the information, the information submitted is, to the best of my knowledge and belief, true, accurate, and complete."

Signature of Inspector

Printed Name and Title

Date

9 TROTTER DRIVE

A SITE PLAN

IN

MEDWAY, MASSACHUSETTS

(NORFOLK COUNTY)

OWNER:

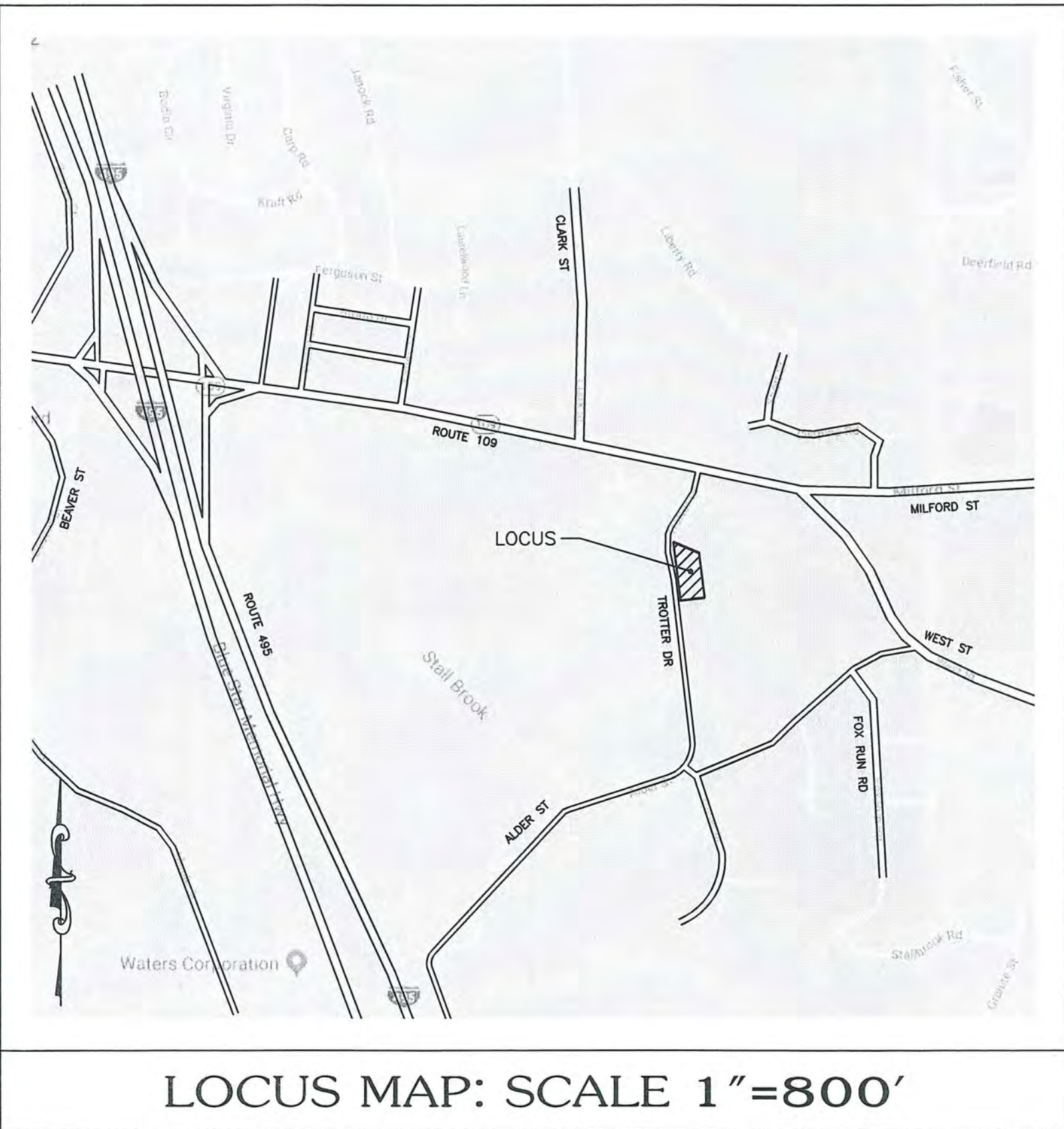
MARGUERITE K. MELE
203 MAIN STREET
MEDWAY, MASSACHUSETTS 02053

APPLICANT:

CTS PROPERTY MANAGEMENT
430 FRANKLIN VILLAGE DRIVE - SUITE 177
FRANKLIN, MASSACHUSETTS 02038

ENGINEER:

ENGINEERING DESIGN CONSULTANTS, INC.
32 TURNPIKE ROAD
SOUTHBOROUGH, MASSACHUSETTS 01772



MARCH 26, 2018

REVISED: JUNE 12, 2018

TOWN OF MEDWAY ASSESSORS ID: 54-004

ZONING DISTRICT: W1

WAIVERS FROM SITE PLAN RULES AND REGULATIONS

- 1) SECTION 204-3 PLANNING BOARD SUBMITTALS, A. 7. DEVELOPMENT IMPACT REPORT
- 2) SECTION 204-4 STANDARDS FOR SITE PLAN PREPARATION - B
- 3) SECTION 204-5 SITE PLAN CONTENTS C. 3. EXISTING LANDSCAPE INVENTORY
- 4) SECTION 204-5 SITE PLAN CONTENTS D. 7. REGISTERED LANDSCAPE ARCHITECT
- 5) SECTION 204-5 SITE PLAN CONTENTS D. 9. COLOR RENDERINGS
- 6) SECTION 205-3 TRAFFIC, B. VERTICAL GRANITE CURB, 6
- 7) SECTION 205-6 PARKING, C. DESIGNATED EMPLOYEE PARKING
- 8) SECTION 205-6 PARKING, G. PARKING SPACES AND STALLS, 3. a
- 9) SECTION 205-6 PARKING, G. PARKING SPACES AND STALLS, 3. b
- 10) SECTION 205-6 PARKING, G. PARKING SPACES AND STALLS, 4. b
- 11) SECTION 205-9 TREES, F. INVENTORY

WAIVER FROM ZONING BYLAW

- 1) SECTION 7.1.1 OFF-STREET PARKING AND LOADING, I. BICYCLE PARKING

SHEET LIST:

SITE PLANS

- 1 COVER SHEET
- 2 EXISTING CONDITIONS
- 3 PROPOSED LAYOUT
- 4 GRADING & UTILITIES
- 5 LIGHTING & LANDSCAPING
- 6 DETAILS

STORMWATER PLANS

- 1 STORMWATER (SWPPP) PLAN

ARCHITECTURAL PLANS

- S-1 FLOOR PLAN
- S-2 ELEVATION PLAN

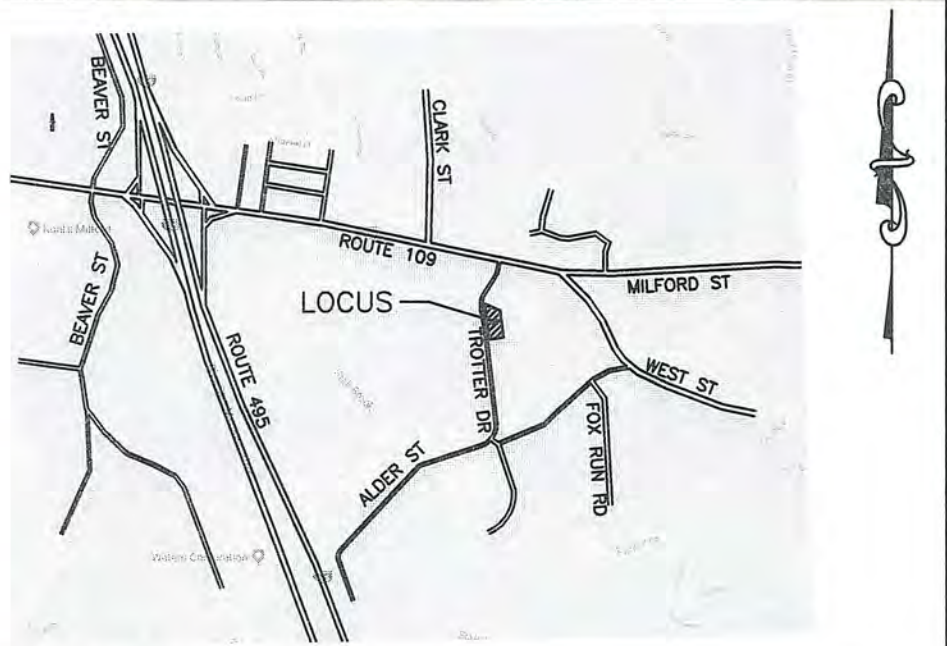
APPROVED BY:
MEDWAY PLANNING & ECONOMIC
DEVELOPMENT BOARD

DATE: _____

CERTIFICATE OF VOTE

SEE CERTIFICATE OF VOTE FOR SITE PLAN APPROVAL DATED, MAY , 2018 FOR PLAN
ENTITLED, '9 TROTTER DRIVE, A SITE PLAN IN MEDWAY MASSACHUSETTS,' FROM THE
MEDWAY PLANNING BOARD, VOTED TO APPROVE WITH STANDARD CONDITIONS.

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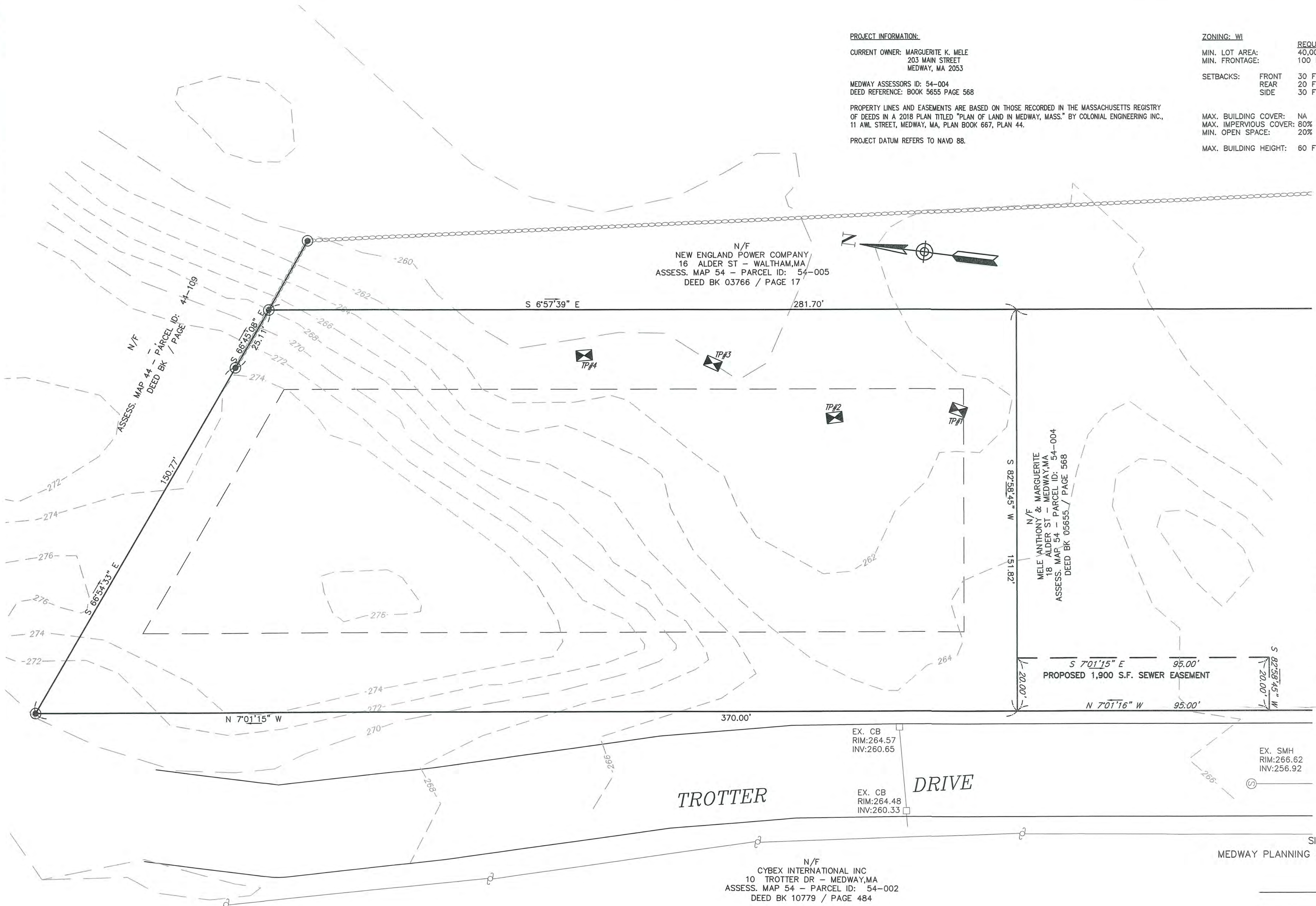
SCALE 1"=2,000'

LOCUS

NOTE:
UNDERGROUND UTILITY LOCATIONS ARE
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TO COMMENCEMENT OF ANY CONSTRUCTION

DIGSAFE IS TO BE NOTIFIED 72 BUSINESS
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CALL DIGSAFE AT 811

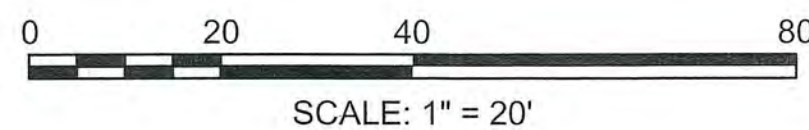


LEGEND

BIT CONC	BITUMINOUS CONCRETE
BORING	BORING
CB	CATCH BASIN
CO	CLEAN OUT
CLF	CHAIN LINK FENCE
CONC	CONCRETE
CW	CROSSWALK
DMH	DRAIN MANHOLE
D	DRAIN LINE
ELEC	ELECTRIC
EMH	ELECTRIC MANHOLE
ECB	EROSION CONTROL BARRIER
FD	FOUNDATION DRAIN
GG	GAS GATE
GAS	GAS LINE
GG	GAS GATE
HC	HANDICAPPED RAMP
HYD	HYDRANT
INV	INVERT
LP	LIGHT POLE
MH	MANHOLE
PVC	POLY-VINYL-CHLORIDE
RCP	REINFORCED CONCRETE PIPE
RD	ROOF DRAIN
S	SEWER LINE
SMH	SEWER MANHOLE
S	SEWER
SD/DH	STONE BOUND/DRILLHOLE
TELE	TELEPHONE LINE
TMH	TELEPHONE MANHOLE
TP	TEST PIT
TRANS	TRANSFORMER
VGC	VERTICAL GRANITE CURB
WG	WATER GATE
W	WATER LINE
W5	WETLAND FLAG

SITE PLAN REVIEW
MEDWAY PLANNING & ECONOMIC DEVELOPMENT BOARD

DEEP HOLE #	TP-1	ELEV.	DEEP HOLE #	TP-2	ELEV.	DEEP HOLE #	TP-3	ELEV.	DEEP HOLE #	TP-4	ELEV.
	GROUND			GROUND			GROUND			GROUND	
6"	A	261.0	6"	A	260.4	6"	A	259.5	6"	A	259.9
24"	Bw	259.5	24"	Bw	258.9	24"	Bw	258.0	24"	Bw	258.4
90"	C	254.0	84"	C	253.9	78"	C	253.5	78"	C	253.9
MOTTILING: 72" 5YR5/6 255.5			MOTTILING: 66" 5YR5/6 255.4			MOTTILING: 56" 5YR5/6 255.3			MOTTILING: 60" 5YR5/6 255.4		
PERCOLATION TEST: N.A.			PERCOLATION TEST: N.A.			PERCOLATION TEST: N.A.			PERCOLATION TEST: N.A.		
SOIL CLASS: CLASS I			SOIL CLASS: CLASS I			SOIL CLASS: CLASS I			SOIL CLASS: CLASS I		



SCALE: 1" = 20'

CERTIFICATE OF VOTE

SEE CERTIFICATE OF VOTE FOR SITE PLAN APPROVAL DATED, MAY 18, 2018 FOR PLAN
ENTITLED, '9 TROTTER DRIVE, A SITE PLAN IN MEDWAY MASSACHUSETTS,' FROM THE
MEDWAY PLANNING BOARD, VOTED TO APPROVE WITH STANDARD CONDITIONS.

DATE: _____

ENGINEERING DESIGN CONSULTANTS, INC. SHALL NOT BE
RESPONSIBLE FOR CONSTRUCTION MEANS, METHODS,
TECHNIQUES, OR PROCEDURES UTILIZED BY THE
CONTRACTOR, NOR FOR THE SAFETY OF PUBLIC OR
CONTRACTOR'S EMPLOYEES; OR FOR THE FAILURE OF THE
CONTRACTOR TO CARRY OUT THE WORK IN ACCORDANCE
WITH THE CONTRACT DOCUMENTS.

ENGINEERING DESIGN CONSULTANTS, INC.'S LIABILITY FOR
THIS PLAN IS LIMITED TO THE EXTENT OF ITS FEE LESS
THIRD PARTY COSTS.

DRAWN BY: ZRB
CHECKED BY: PSB
APPROVED BY: WML

REVISIONS:
1 6/12/18 PEER REVIEW COMMENTS
2 3/26/18 SITE PLAN REVIEW
3 DATE DESCRIPTION



PREPARED BY:
Engineering Design Consultants, Inc.
32 Turnpike Road
Southborough, Massachusetts
ph: (508) 480-0225 fax: (508) 832-5781

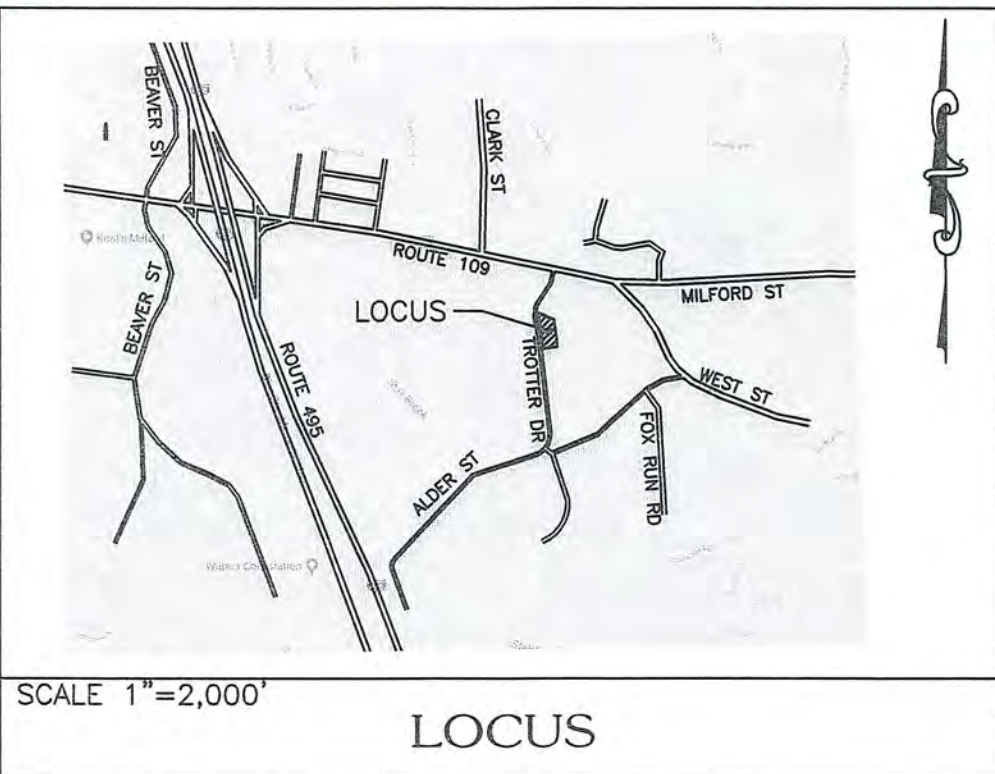
PROJECT:
SITE PLAN
9 TROTTER DRIVE
MEDWAY, MASSACHUSETTS 02053

TITLE:
EXISTING CONDITIONS
SITE PLAN
OWNER: MARGUERITE K. MELE
203 MAIN STREET
MEDWAY, MASSACHUSETTS 02053

APPLICANT: CTS PROPERTY MANAGEMENT
430 FRANKLIN VILLAGE DRIVE - SUITE 177
FRANKLIN, MASSACHUSETTS 02038

FILE NO: 3583
EXISTING CONDITIONS
DATE: MARCH 26, 2018
PLAN NUMBER: 2 of 6
2

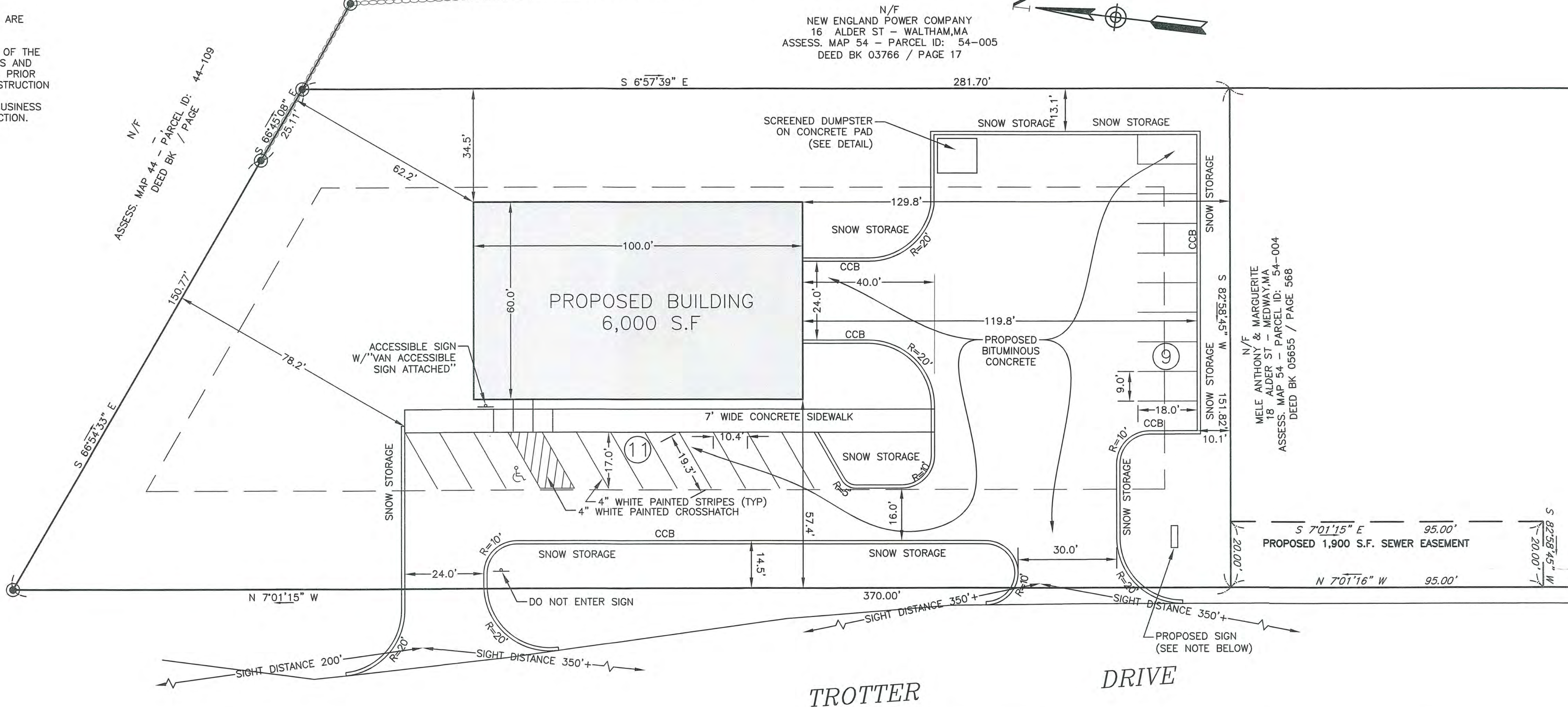
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NOTE:
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TO COMMENCEMENT OF ANY CONSTRUCTION

DIGSAFE IS TO BE NOTIFIED 72 BUSINESS
HOURS IN ADVANCE OF CONSTRUCTION.
CALL DIGSAFE AT 811



PARKING SCHEDULE:

REQUIRED: 1 SPACE PER 300 SF OF OFFICE = $\frac{1}{300} \times 264$ = 1 SPACE
1 SPACE PER 2 EMPLOYEES = $\frac{1}{2} \times 10$ = 5 SPACES
1 SPACE PER 1,000 SF OF BUILDING = $\frac{1}{1000} \times 6,000$ = 6 SPACES
TOTAL OF 12 SPACES REQUIRED.
PROVIDED: 20 SPACES ARE PROVIDED INCLUDING 1 "VAN ACCESSIBLE" SPACE.

PROJECT INFORMATION:

CURRENT OWNER: MARGUERITE K. MELE
203 MAIN STREET
MEDWAY, MA 02053

MEDWAY ASSESSORS ID: 54-004

PROPERTY LINES AND EASEMENTS ARE BASED ON THOSE RECORDED IN THE MASSACHUSETTS REGISTRY
OF DEEDS IN A 2018 PLAN TITLED "PLAN OF LAND IN MEDWAY, MASS." BY COLONIAL ENGINEERING INC.,
11 AUL STREET, MEDWAY, MA, PLAN BOOK 667, PLAN 44.

PROJECT DATUM REFERS TO NAVD 88.

ZONING: W1

	REQUIRED	EXISTING	PROPOSED
MIN. LOT AREA:	40,000 S.F.	49,528±S.F.	57.4± FT.
MIN. FRONTAGE:	100 FT.	370.00±FT.	34.5± FT.
SETBACKS:			
FRONT	30 FT.	NA	57.4± FT.
REAR	20 FT.	NA	34.5± FT.
SIDE	30 FT.	NA	62.2± FT.
MAX. BUILDING COVER:	NA	0%	13%
MAX. IMPERVIOUS COVER:	80%	0%	48%
MIN. OPEN SPACE:	20%	100%	52%
MAX. BUILDING HEIGHT:	60 FT.	NA	<60 FT.

LEGEND

BIT CONC	BITUMINOUS CONCRETE
BORING	BORING
CB	CATCHBASIN
CCB	CAPE COD BERM
CO	CLEAN OUT
CLF	CHAIN LINK FENCE
CONC	CONCRETE
CW	CROSSWALK
DMH	DRAIN MANHOLE
D	DRAIN LINE
ELEC	ELECTRIC
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S	SEWER LINE
SMH	SEWER MANHOLE
SG	SIGN
SB/DH	STONE BOUND/DRILLHOLE
TELE	TELEPHONE LINE
TMH	TELEPHONE MANHOLE
TRANS	TRANSFORMER
VGC	VERTICAL GRANITE CURB
WG	WATER GATE
W	WATER LINE
W5	WETLAND FLAG

SITE PLAN REVIEW
MEDWAY PLANNING & ECONOMIC DEVELOPMENT BOARD

DATE: _____

SIGN NOTE:

PROPOSED SIGN SHALL BE 6'-8" LONG BY 3'-0" HIGH BY 2'-0" MONUMENT
SIGN SETBACK 10'-0" FROM PROPERTY LINES. BASIS FOR SIGN DESIGN IS WEST
INDUSTRIAL GROUND MONUMENT SIGN AS ALLOWED PER SIGN BYLAW HAVING
MAXIMUM SIZE OF 20.0 SF. PER SIDE FOR A 2 SIDED SIGN. A TWO SIDED 20.0
SF. PER SIDE SIGN IS PROPOSED.

SNOW STORAGE IS LIMITED ALONG THE PERIMETER OF THE PARKING
AREA LIMITS AND AS SUCH WILL REQUIRE PERIODIC REMOVAL SHOULD
SNOWFALL EVENTS EXCEED STORAGE LIMITS. WHEN SNOW ACCUMULATION
EXCEEDS STORAGE CAPACITY IT SHALL BE REMOVED FROM THE SITE
AND DISPOSED OF WITH A LICENSED FACILITY.

CERTIFICATE OF VOTE

SEE CERTIFICATE OF VOTE FOR SITE PLAN APPROVAL DATED, MAY , 2018 FOR PLAN
ENTITLED, '9 TROTTER DRIVE, A SITE PLAN IN MEDWAY MASSACHUSETTS,' FROM THE
MEDWAY PLANNING BOARD, VOTED TO APPROVE WITH STANDARD CONDITIONS.

ALL RIGHTS RESERVED

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ENGINEERING DESIGN CONSULTANTS, INC.'S LIABILITY FOR
THIS PLAN IS LIMITED TO THE EXTENT OF ITS FEE LESS
THIRD PARTY COSTS.

DRAWN BY: ZRB
CHECKED BY: PSB
APPROVED BY: WML

REVISIONS:
1 6/12/18 PEER REVIEW COMMENTS
2 3/26/18 SITE PLAN REVIEW
3 6-12-18 DESCRIPTION



PREPARED BY:



Engineering Design Consultants, Inc.
32 Turnpike Road
Southborough, Massachusetts
ph:(508) 480-0225 fax:(800)832-5781

PROJECT:

SITE PLAN
9 TROTTER DRIVE
MEDWAY, MASSACHUSETTS 02053

TITLE:

PROPOSED LAYOUT
SITE PLAN

OWNER:

MARGUERITE K. MELE
203 MAIN STREET
MEDWAY, MASSACHUSETTS 02053

APPLICANT:

CTS PROPERTY MANAGEMENT
430 FRANKLIN VILLAGE DRIVE - SUITE 177
FRANKLIN, MASSACHUSETTS 02038

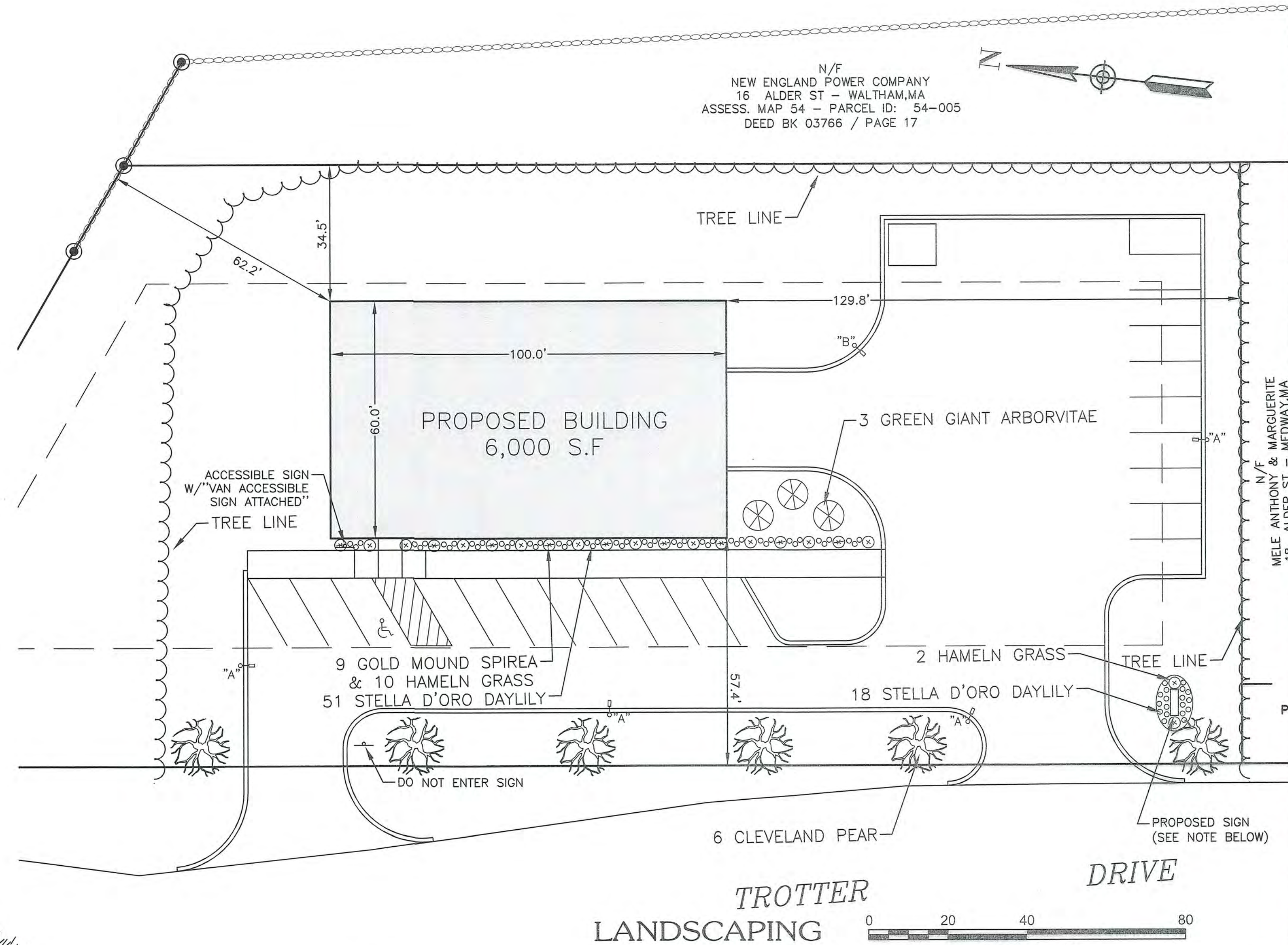
FILE NO: 3583

PROPOSED LAYOUT

DATE: MARCH 26, 2018

PLAN NUMBER:

3 of 6
3



Statistics						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Parking Area	+	1.5 fc	3.0 fc	0.5 fc	6.0:1	3.0:1
Spill	+	0.1 fc	2.9 fc	0.0 fc	N/A	N/A

DIGSAFE IS TO BE NOTIFIED 72 BUSINESS
HOURS IN ADVANCE OF CONSTRUCTION.
CALL DIGSAFE AT 811

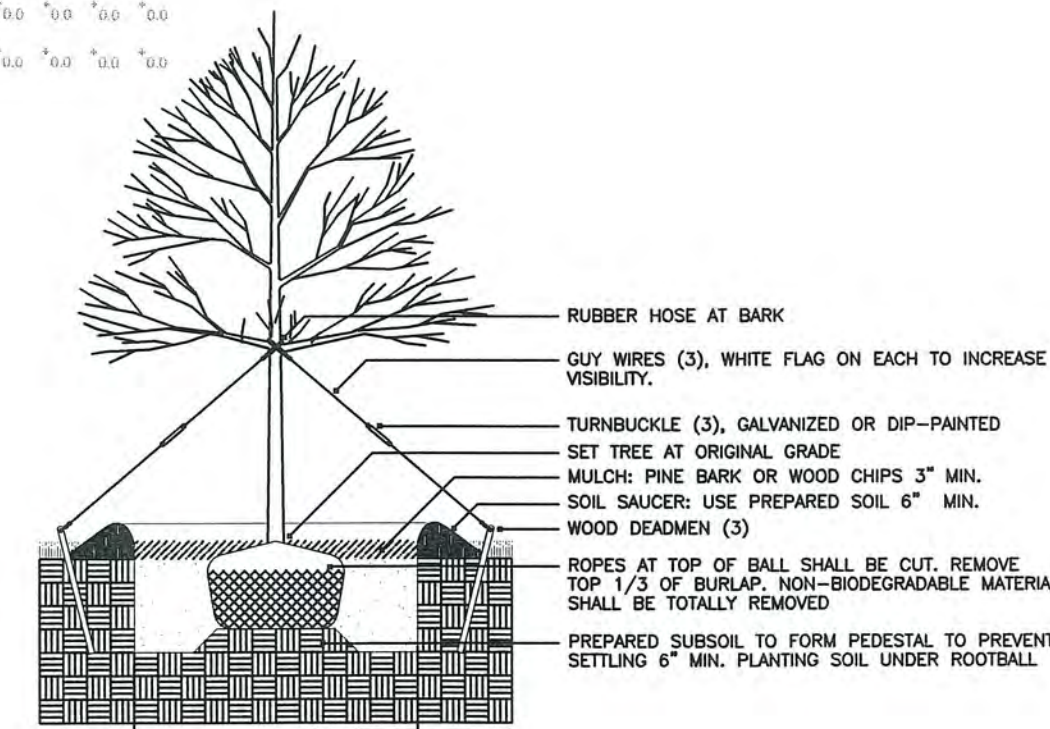


Diagram illustrating the construction of a tree in a burlap ball, showing the following components and instructions:

- THIN BRANCHES BY 1/3 RETAINING NORMAL PLANT SHAPE.
- BARK MULCH 3" MIN.
- CREATE SAUCER WITH TOPSOIL 6" MIN.
- ROPES AT TOP OF BALL SHALL BE CUT. REMOVE ALL OF BURLAP.
- GENTLY COMPACTED TOPSOIL MIXTURE
- TAMPED ADMIXTURE BACKFILL.
- 2X BALL DIA. MIN.

BALL AND BURLAP SHRUB PLANTING

Percent Passing
100
70-80
50-80
15-40
0-3

PLANT LIST			
QUANTITY	COMMON NAME	LATIN NAME	SIZE
TREES			
6	CLEVELAND PEAR	PYRUS CALLERYANA	2-2.5" CAL.
3	GREEN GIANT ARBORVITAE	THUJA PLICATA	5'-6" B&B
SHRUBS			
9	GOLD MOUND SPIREA	SPIREA JAPONICA 'GOLD MOUND'	2 GALLON
10	DWARF HAMELEN GRASS	PENNISTUM ALOPEUROIDES	2 GALLON
PERENNIALS			
51	STELLA D'ORO SPRUCE	HEMEROCALLIS 'STELLA D'ORO'	1 GALLON

PROPOSED SIGN SHALL BE 6'-8" LONG BY 3'-0" HIGH BY 2'-0" MONUMENT
SIGN SETBACK 10'-0" FROM PROPERTY LINES. BASIS FOR SIGN DESIGN IS WEST
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MAXIMUM SIZE OF 20.0 SF. PER SIDE FOR A 2 SIDED SIGN. A TWO SIDED 20.0
SF. PER SIDE SIGN IS PROPOSED.

SEE CERTIFICATE OF VOTE FOR SITE PLAN APPROVAL DATED, _____, 2018 FOR PLAN ENTITLED, '9 TROTTER DRIVE, A SITE PLAN IN MEDWAY MASSACHUSETTS,' FROM THE MEDWAY PLANNING BOARD, VOTED TO APPROVE WITH STANDARD CONDITIONS.

DATE: _____

Engineering Design Consultants
32 Turnpike Road
Southborough, Massachusetts
ph:(508) 480-0225 fax:(800)832-5781

SITE PLAN
9 TROTTER DRIVE
MEDWAY, MASSACHUSETTS 02053

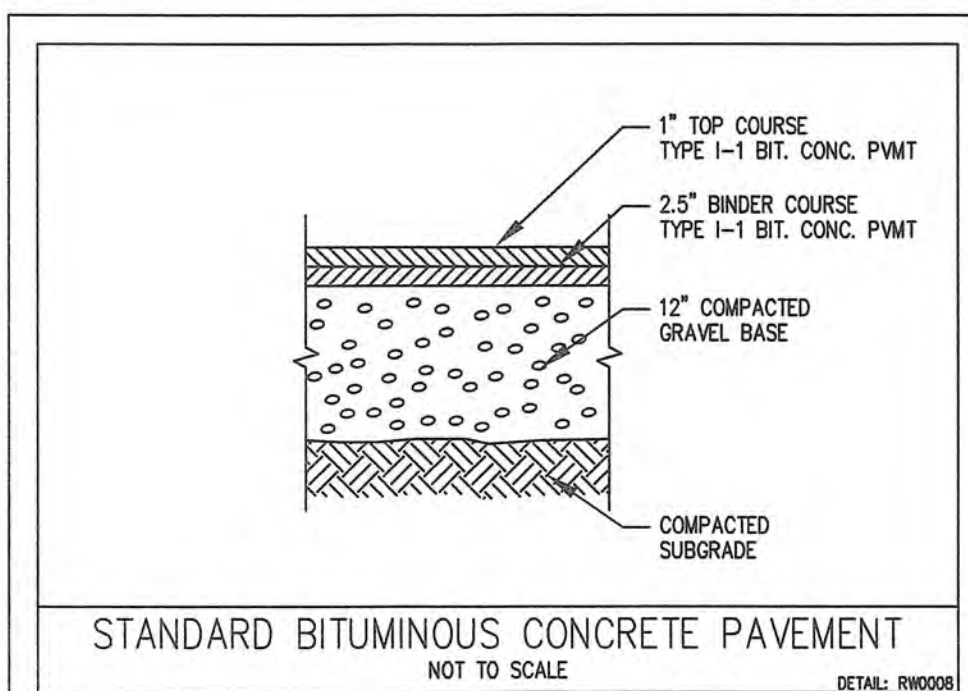
LIGHTING & LANDSCAPING SITE PLAN

OWNER: MARGUERITE K. MELE
203 MAIN STREET
MEDWAY, MASSACHUSETTS 02053

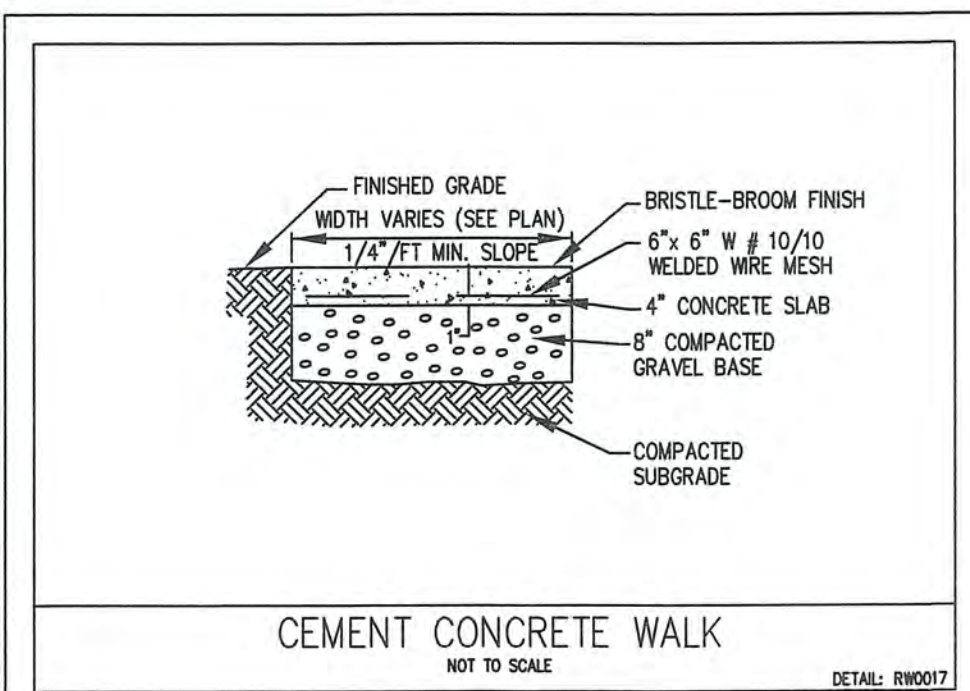
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FILE NO.: 3583
LIGHTING & LANDSCAPING
DATE: MARCH 26, 2018
PLAN NUMBER:
5 OF 6
5

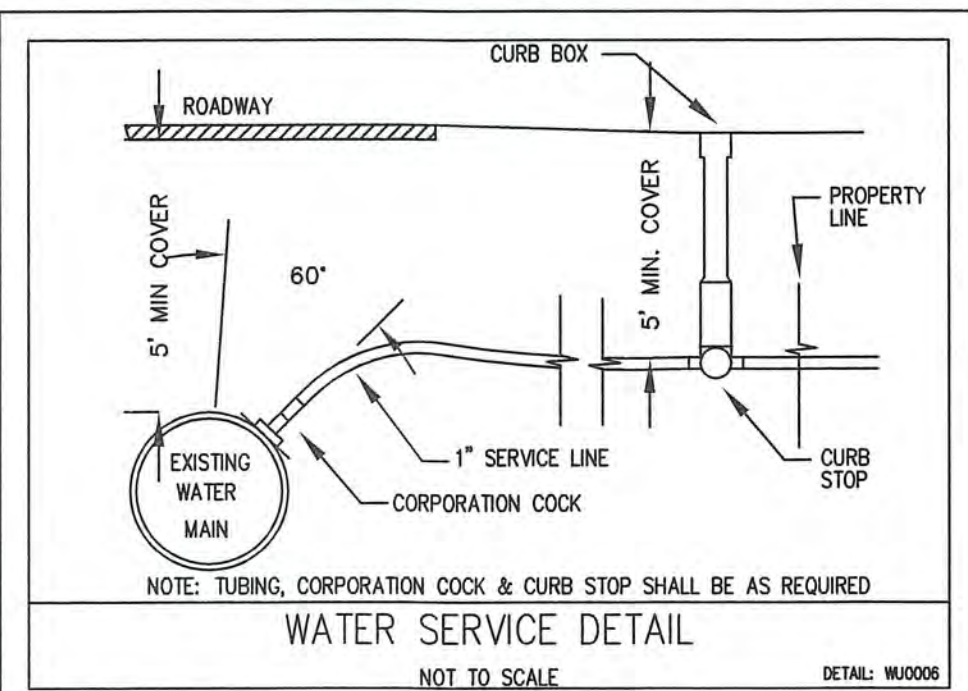
\\SPKRDG\JOBS\JOBS\AUTOCAD FILES\DWG\0003.DWG



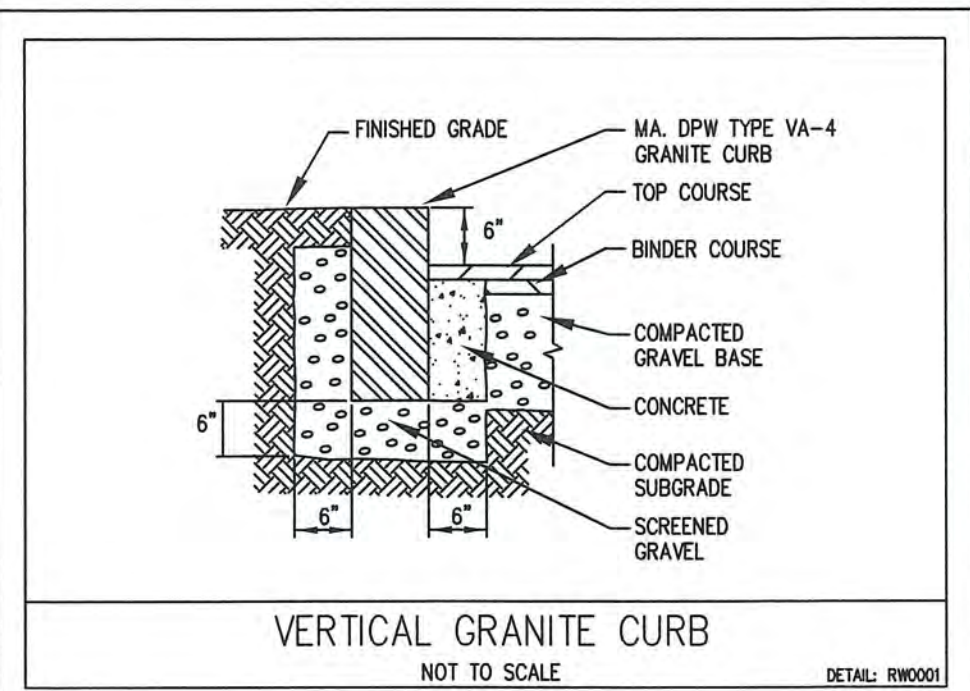
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NOT TO SCALE
DETAIL: RW0008



CEMENT CONCRETE WALK
NOT TO SCALE
DETAIL: RW0017



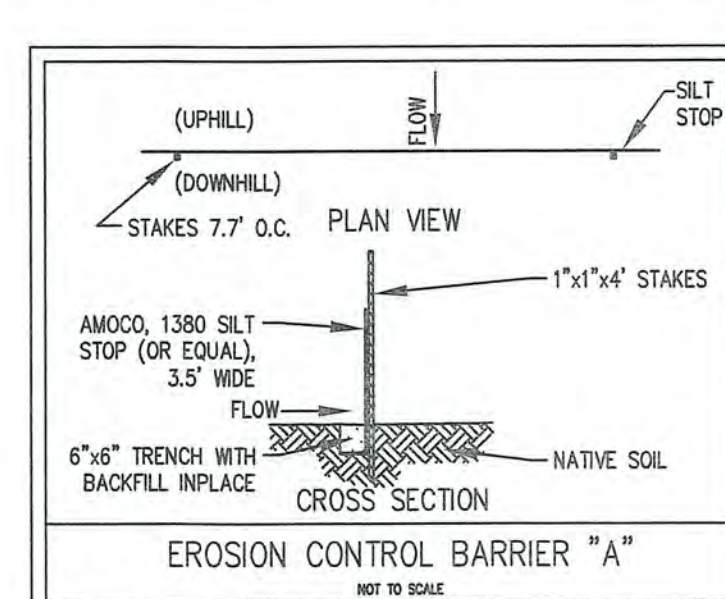
WATER SERVICE DETAIL
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DETAIL: RW0006



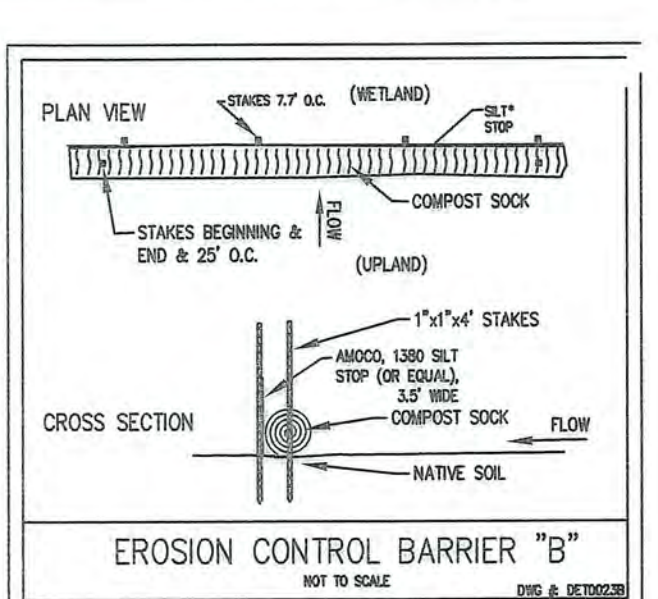
VERTICAL GRANITE CURB
NOT TO SCALE
DETAIL: RW0001

1. EROSION CONTROL MEASURES (SEE DETAILS) SHALL BE INSTALLED PRIOR TO COMMENCEMENT OF CONSTRUCTION AS INDICATED.
2. MEASURES SHALL BE TAKEN TO LIMIT THE TRACKING OF MATERIAL FROM THE SITE ONTO RUSTIC ROAD AND SUMMER STREET.
3. DUST SHALL BE CONTROLLED BY SPRINKLING OR OTHER APPROVED MEASURES AS NECESSARY.
4. THROUGHOUT CONSTRUCTION, ALL DISTURBED AREAS SHALL BE LOAMED AND SEEDED AND MULCHED AS SOON AS PRACTICABLE IN ORDER TO AVOID OVERLAND TRANSPORT OF SEDIMENT.
5. EROSION CONTROL MEASURES ARE TO BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD AND PARTICULARLY AFTER SIGNIFICANT RAINFALL. DILAPIDATED HAYBALES AND SILT STOP SHALL BE REPLACED AS REQUIRED TO MAINTAIN THE INTEGRITY OF THE BARRIER.
6. WITH SLOPES STABILIZED AND FINAL INSPECTION COMPLETE, THE EROSION CONTROL BARRIER SHALL BE REMOVED AND PROPERLY DISPOSED OFF-SITE.

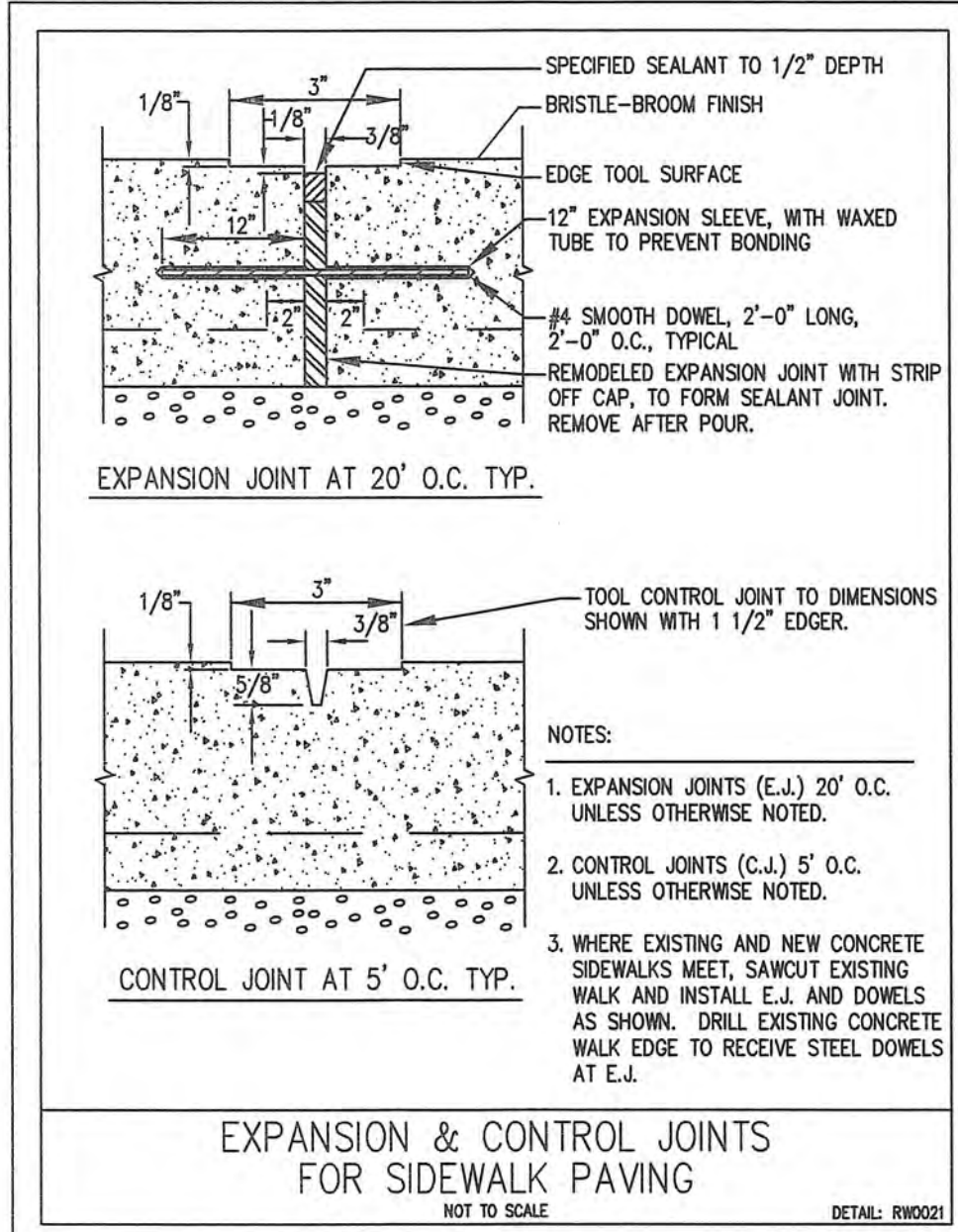
SEDIMENTATION & EROSION CONTROL METHODS & PROCEDURES
DETAIL: EC0002



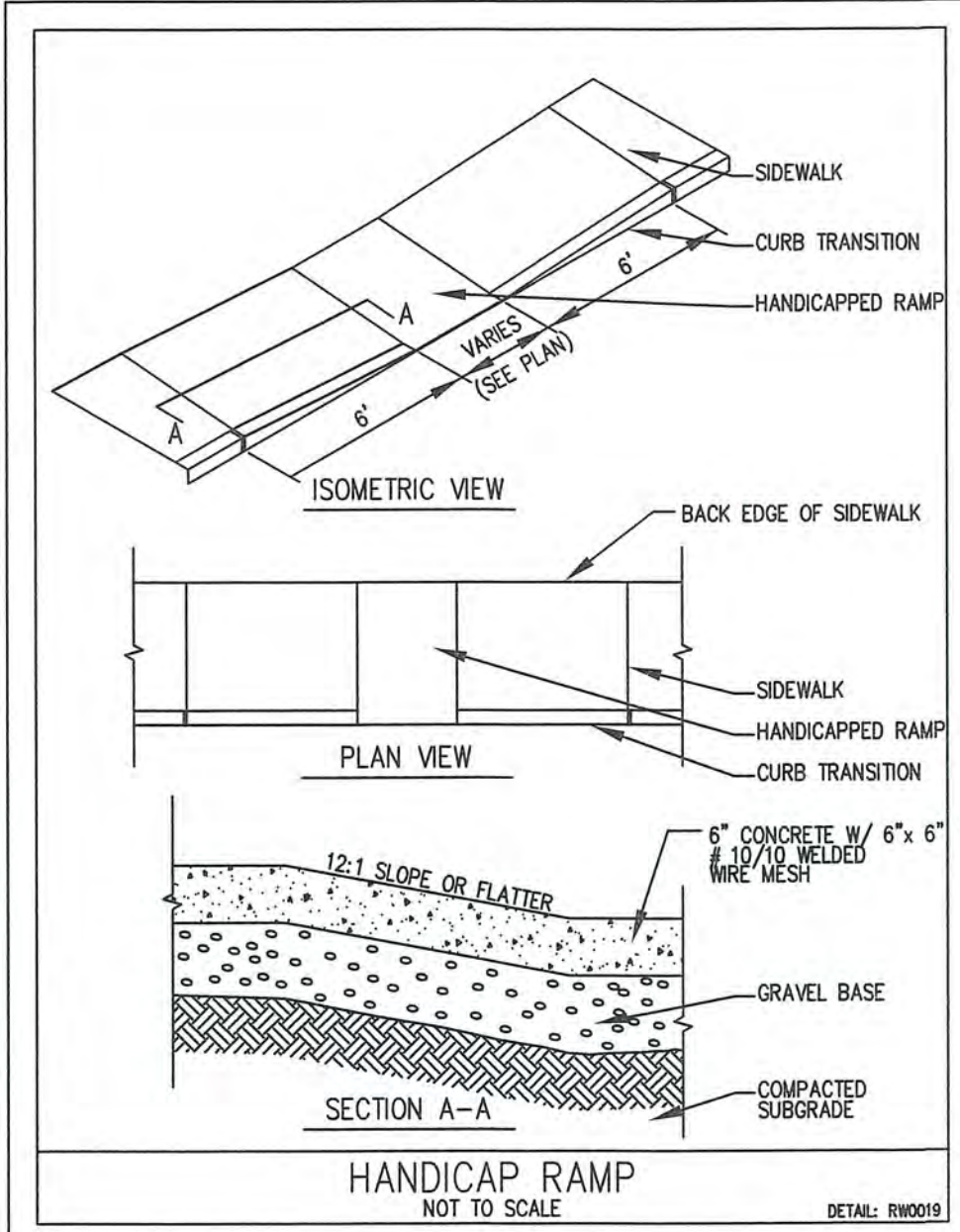
EROSION CONTROL BARRIER "A"
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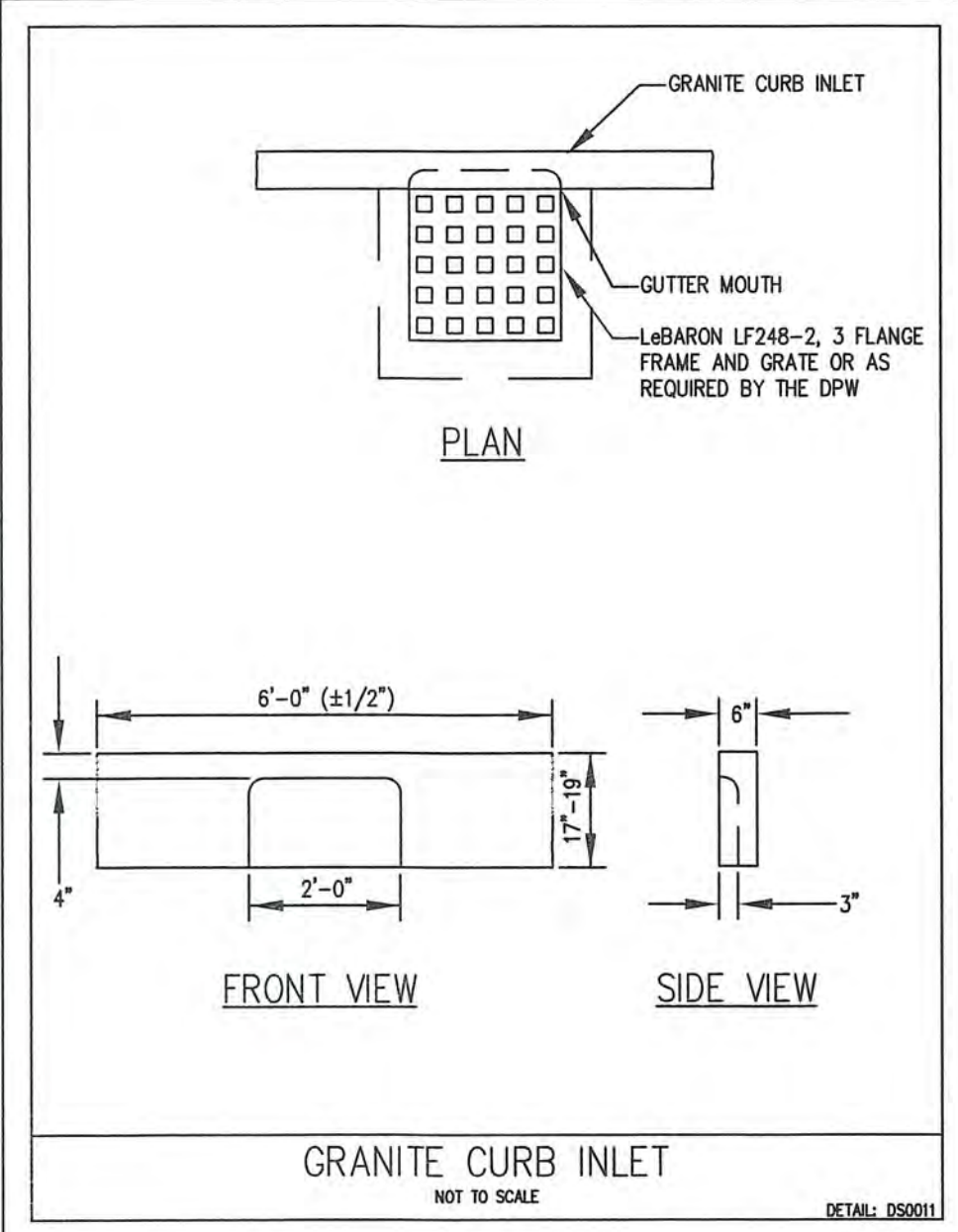
EROSION CONTROL BARRIER "B"
NOT TO SCALE
DES. & DETAIL: DSG



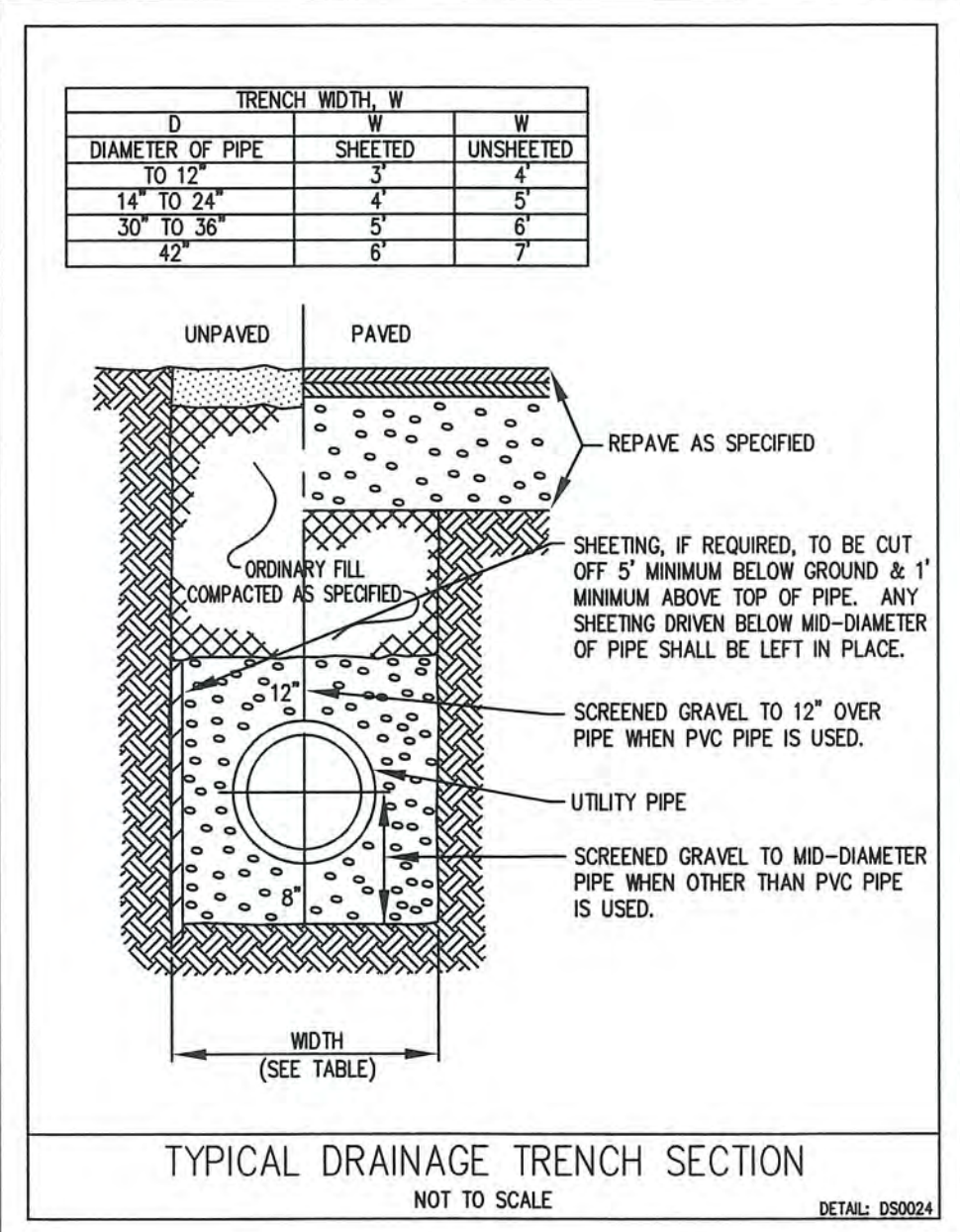
EXPANSION & CONTROL JOINTS FOR SIDEWALK PAVING
NOT TO SCALE
DETAIL: RW0021



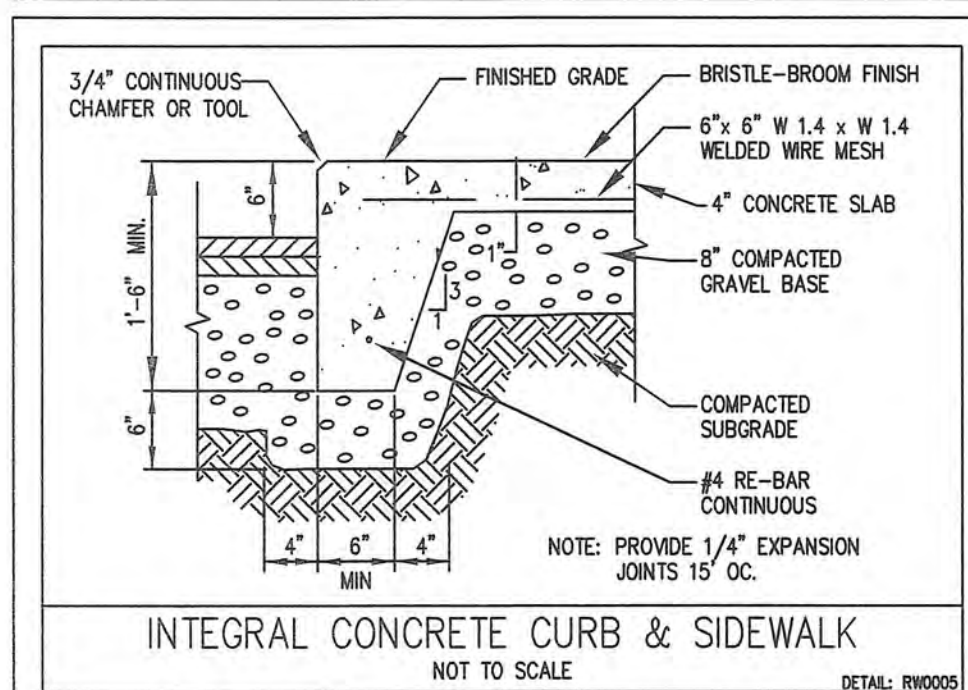
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DETAIL: RW0019



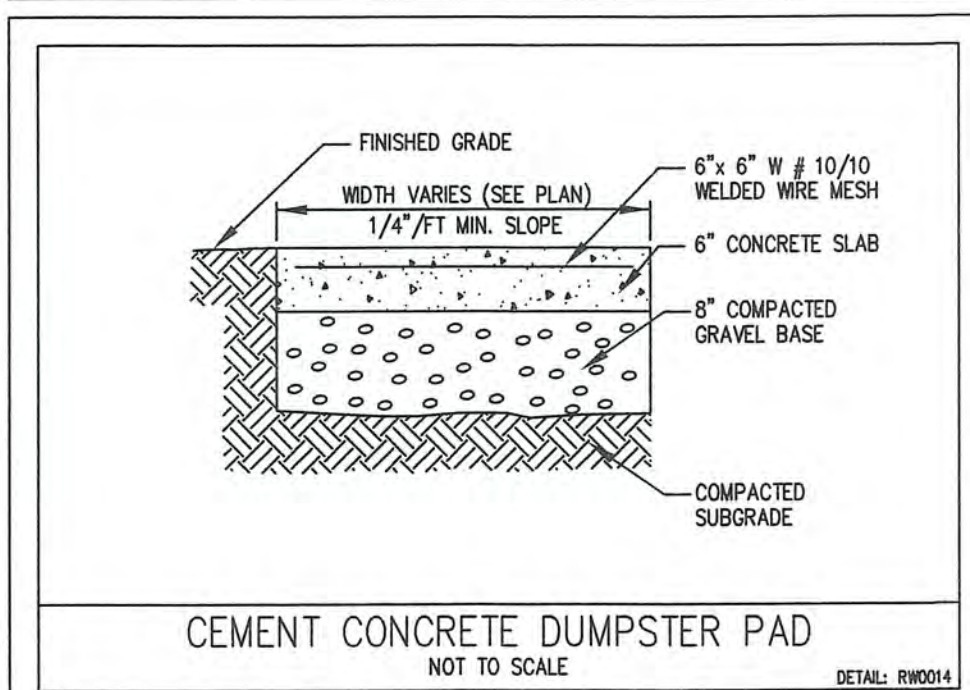
GRANITE CURB INLET
NOT TO SCALE
DETAIL: RW0001



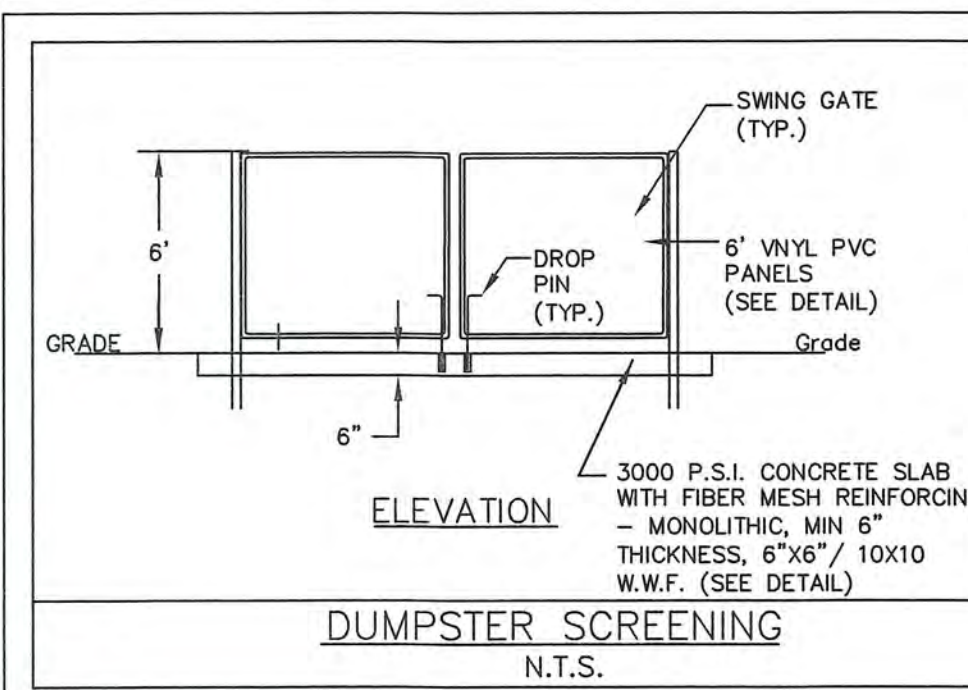
TYPICAL DRAINAGE TRENCH SECTION
NOT TO SCALE
DETAIL: DS0004



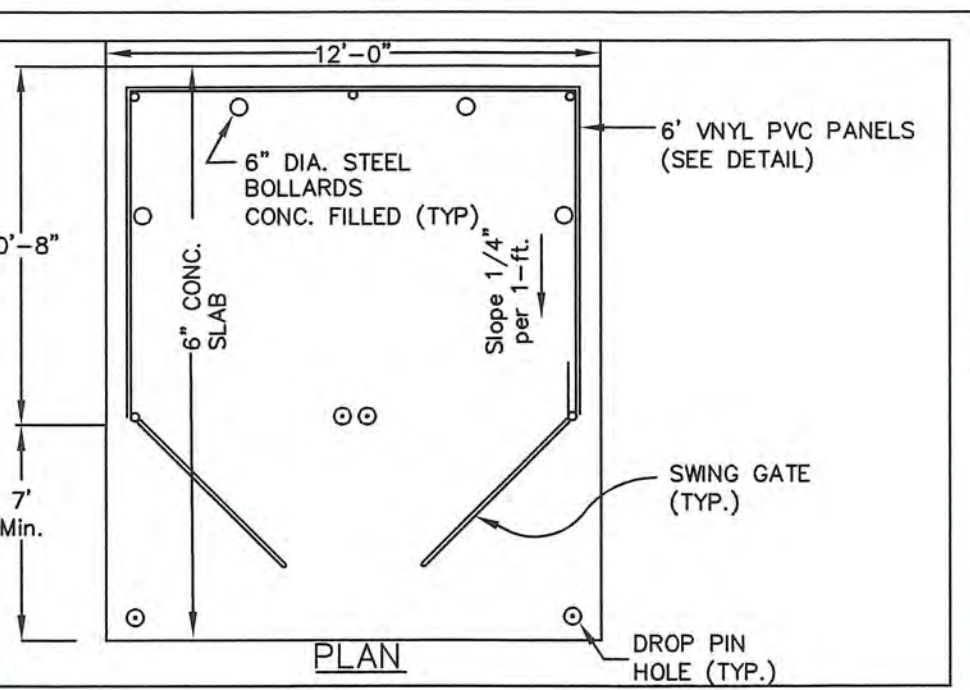
INTEGRAL CONCRETE CURB & SIDEWALK
NOT TO SCALE
DETAIL: RW0005



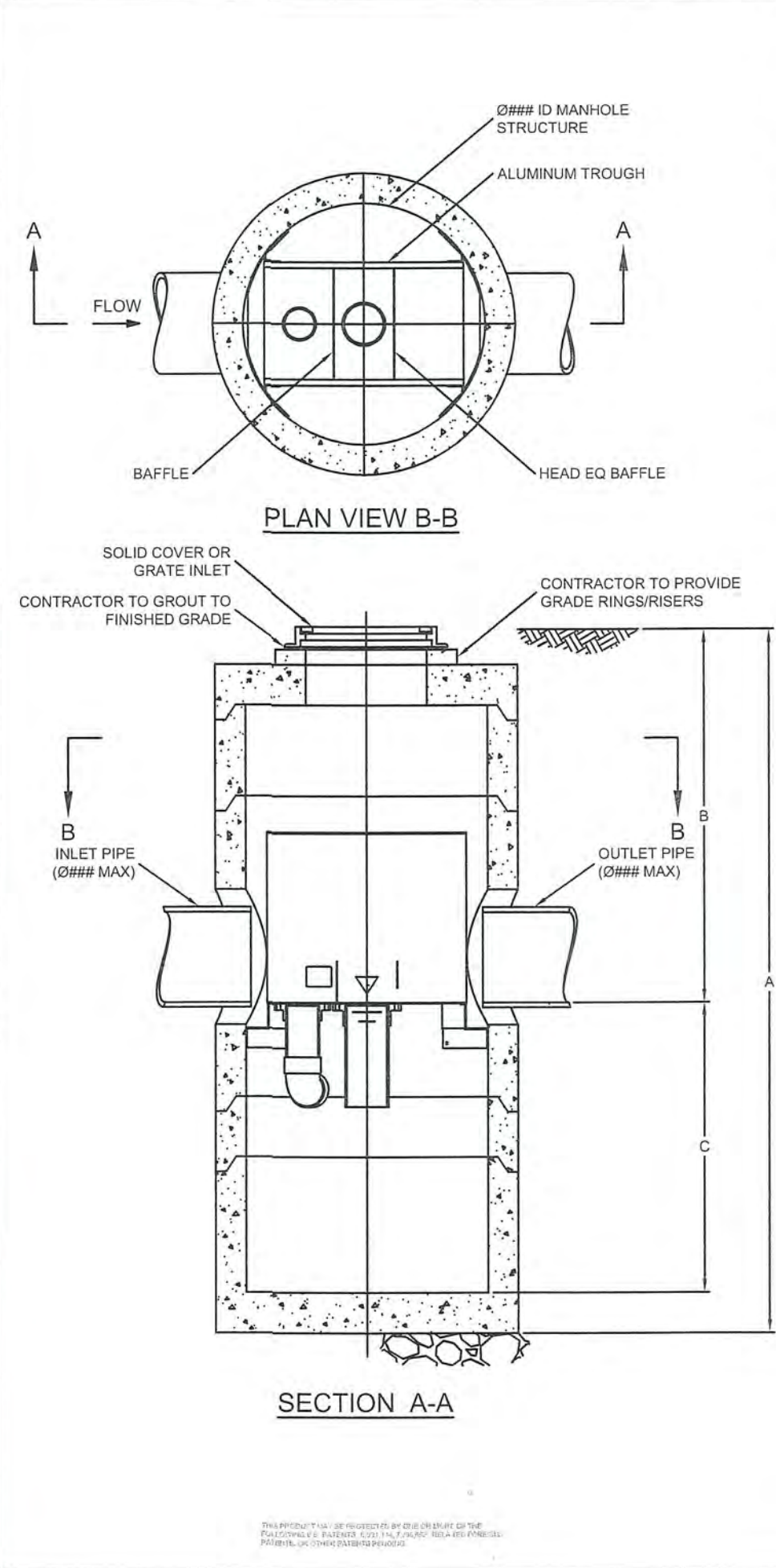
CEMENT CONCRETE DUMPSTER PAD
NOT TO SCALE
DETAIL: RW0014



DUMPSTER SCREENING
N.T.S.
DETAIL: DS0001



TYPICAL DRAINAGE TRENCH SECTION
NOT TO SCALE
DETAIL: DS0004



VORTSENTRY HS DESIGN NOTES

RATED TREATMENT CAPACITY IS SHOWN IN THE TABLE BELOW, OR PER LOCAL REGULATIONS. MAXIMUM HYDRAULIC INTERNAL BYPASS CAPACITY VARIES. CONTACT YOUR CONTECH REPRESENTATIVE FOR ADDITIONAL INFORMATION.

THE STANDARD SOLID COVER CONFIGURATION IS SHOWN. ALTERNATE CONFIGURATIONS ARE AVAILABLE AND ARE LISTED BELOW.

CONFIGURATION OPTION DESCRIPTION
GRATE INLET (NO INLET PIPE)
GRATE INLET WITH INLET PIPE

VORTSENTRY HS GENERAL INFORMATION

Model	Manhole Diameter (ID)	Total Treatment Flow Rate	Typical Total Distance Rim to Outside Bottom		Typical Depth Below Invert (inside)		Approximate Minimum Distance Rim to Invert	Maximum Pipe Diameter (ID)						
			FT	M	FT	M								
HS36	3	900	0.55	15.6	10.16	3.10	4.08	1.24	5.58	1702	3.00	0.91	18	450
HS48	4	1200	1.20	34.0	15.23	4.64	0.00	1.83	0.73	2037	2.00	1.22	24	600
HS60	5	1500	2.20	62.2	15.13	4.61	0.00	1.86	0.73	2128	2.00	1.22	30	750
HS72	6	1800	3.70	104.0	10.66	3.23	0.00	0.96	0.46	2700	2.00	1.22	36	900
HS84	7	2100	5.20	155.0	10.23	3.10	0.00	0.96	0.46	2700	2.00	1.22	42	1050
HS96	8	2400	6.40	192.0	10.07	3.06	0.00	0.96	0.46	2700	2.00	1.22	48	1200

SITE SPECIFIC DATA REQUIREMENTS

STRUCTURE ID			
WATER QUALITY FLOW RATE (CFS)			
PEAK FLOW RATE (CFS)			
RETURN PERIOD OF PEAK FLOW (YRS)			
PIPE DATA:	I.E.	MATERIAL	DIAMETER
INLET PIPE			
OUTLET PIPE			
RIM ELEVATION			
ANTI-FLOTATION BALLAST	WIDTH	HEIGHT	
NOTES/SPECIAL REQUIREMENTS:			
* PER ENGINEER OF RECORD			

GENERAL NOTES

- CONTECH TO PROVIDE ALL MATERIALS UNLESS NOTED OTHERWISE.
- DIMENSIONS MARKED WITH () ARE REFERENCE DIMENSIONS. ACTUAL DIMENSIONS MAY VARY.
- FOR FABRICATION DRAWINGS WITH DETAILED STRUCTURE DIMENSIONS AND WEIGHT, PLEASE CONTACT YOUR CONTECH ENGINEERED SOLUTIONS LLC REPRESENTATIVE. www.conteches.com
- VORTSENTRY HS WATER QUALITY STRUCTURE SHALL BE IN ACCORDANCE WITH ALL DESIGN DATA AND INFORMATION CONTAINED IN THIS DRAWING.
- STRUCTURE SHALL MEET AASHTO HS20 AND CASTINGS SHALL MEET AASHTO M306 LOAD RATING, ASSUMING GROUNDWATER ELEVATION AT, OR BELOW, THE OUTLET PIPE INVERT ELEVATION. ENGINEER OF RECORD TO CONFIRM ACTUAL GROUNDWATER ELEVATION.

INSTALLATION NOTES

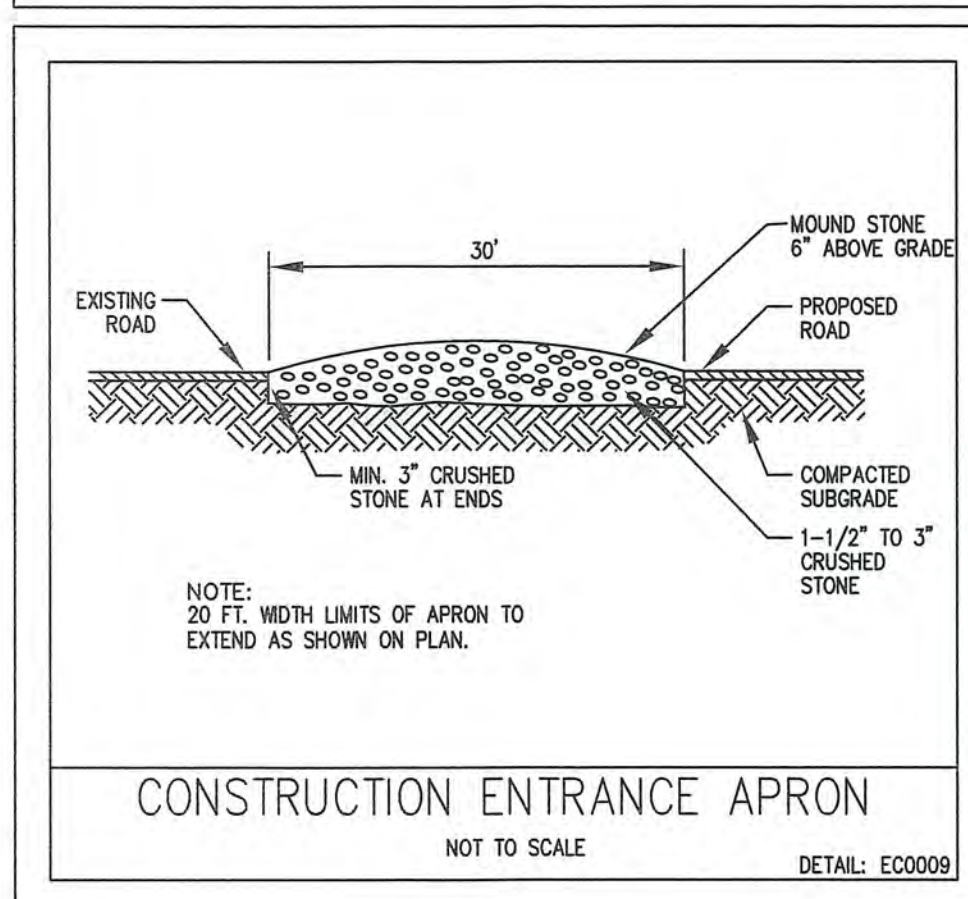
- ANY SUB-BASE, BACKFILL DEPTH, AND/OR ANTI-FLOTATION PROVISIONS ARE SITE-SPECIFIC DESIGN CONSIDERATIONS AND SHALL BE SPECIFIED BY ENGINEER OF RECORD.
- CONTRACTOR TO PROVIDE EQUIPMENT WITH SUFFICIENT LIFTING AND REACH CAPACITY TO LIFT AND SET THE VORTSENTRY HS MANHOLE STRUCTURE (LIFTING CLUTCHES PROVIDED).
- CONTRACTOR TO INSTALL JOINT SEALANT BETWEEN ALL STRUCTURE SECTIONS AND ASSEMBLE STRUCTURE.
- CONTRACTOR TO PROVIDE, INSTALL, AND GROUT PIPES. MATCH PIPE INVERTS WITH ELEVATIONS SHOWN.
- CONTRACTOR TO TAKE APPROPRIATE MEASURES TO ASSURE UNIT IS WATER TIGHT, HOLDING WATER TO FLOWLINE INVERT MINIMUM. IT IS SUGGESTED THAT ALL JOINTS BELOW PIPE INVERTS ARE GROUTED.

CONTECH
ENGINEERED SOLUTIONS LLC

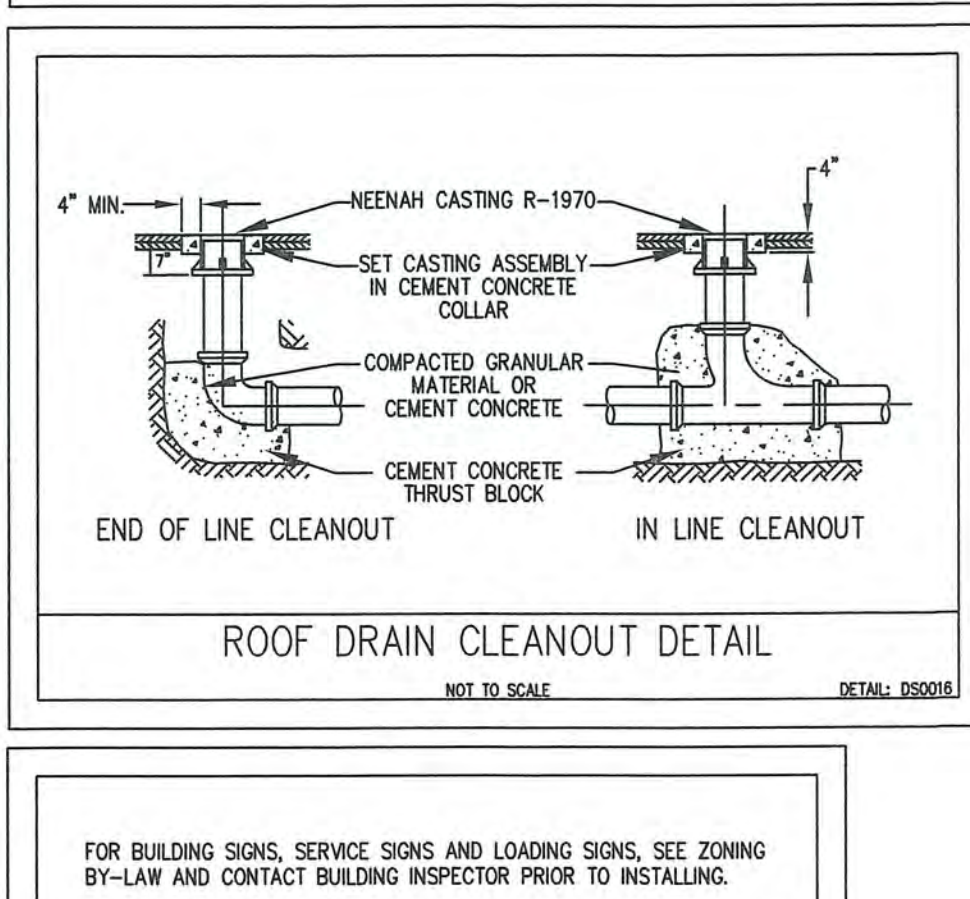
9075 Corporate Plaza Dr., Suite 400, West Chester, OH 43081
937-321-1122 FAX: 937-321-1123

VORTSENTRY HS
STANDARD DETAIL

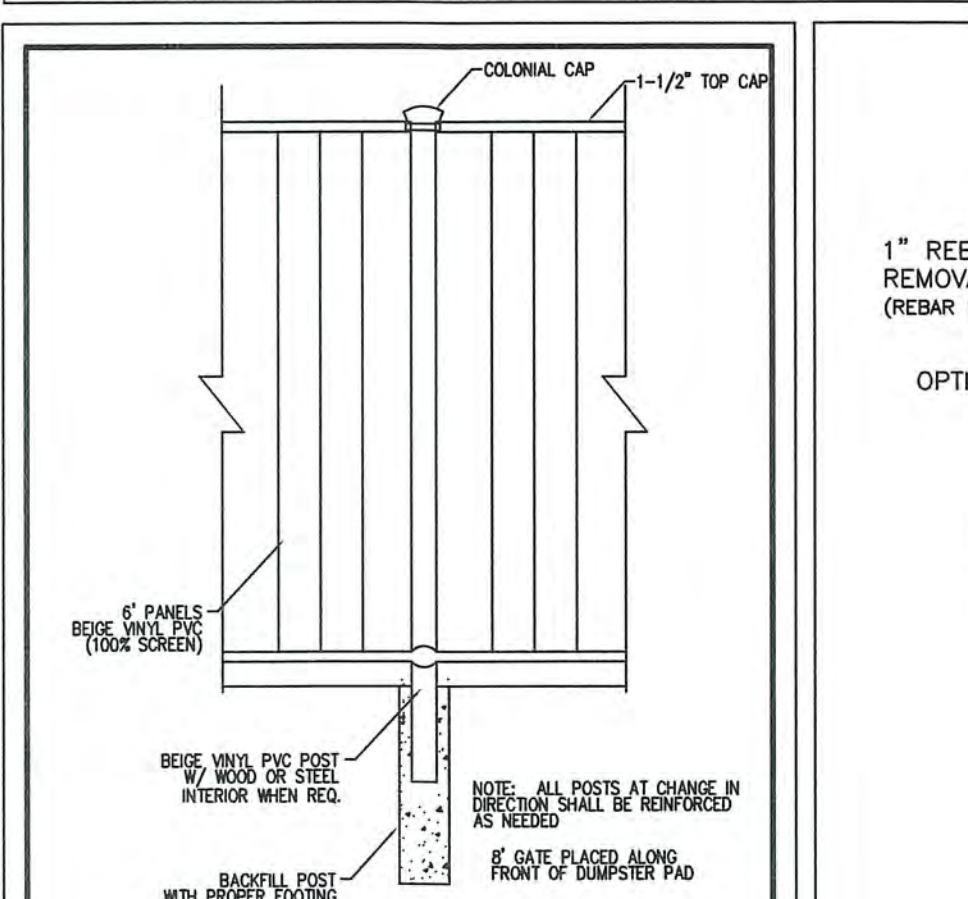
DETAIL: DS0006



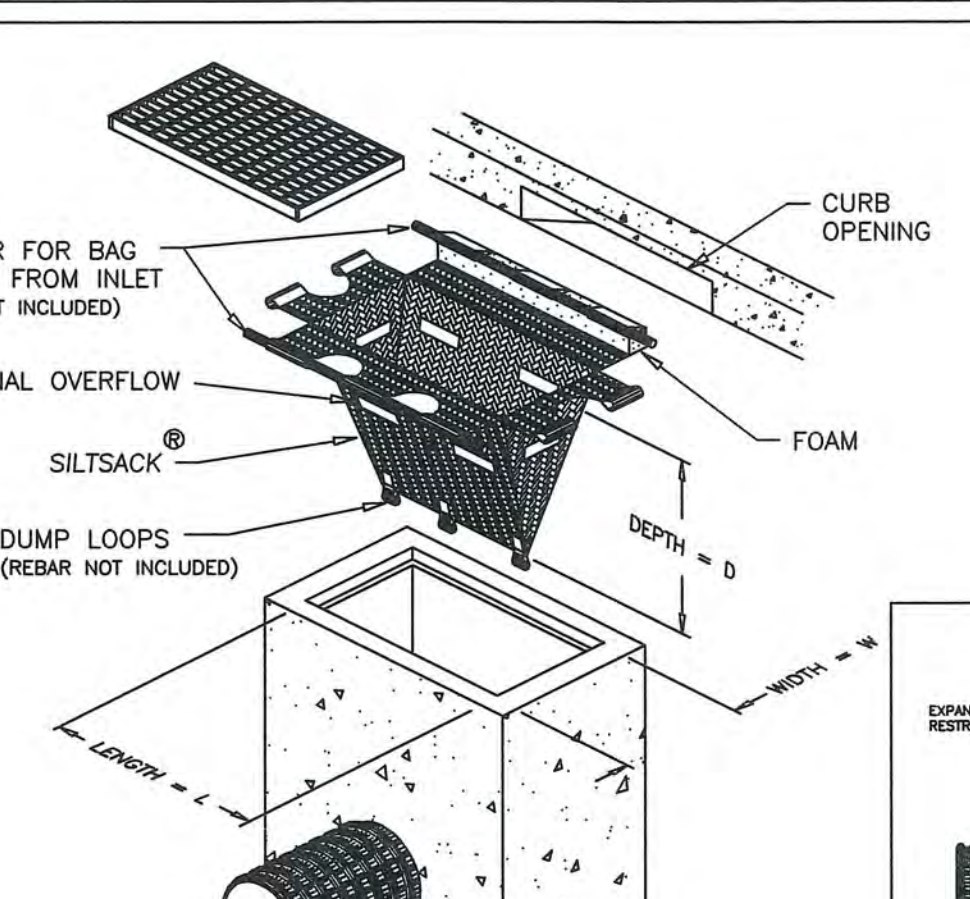
CONSTRUCTION ENTRANCE APRON
NOT TO SCALE
DETAIL: EC0009



ROOF DRAIN CLEANOUT DETAIL
NOT TO SCALE
DETAIL: DS0008



DIRECTIONAL SIGNS
NOT TO SCALE
DES. & DETAIL: DSG



SILTSACK
NOT TO SCALE

SILTSACK® SPECIFICATIONS

NOTE: THE SILTSACK® WILL BE MANUFACTURED FROM A WOVEN POLYPROPYLENE FABRIC THAT MEETS OR EXCEEDS THE FOLLOWING SPECIFICATIONS.

REGULAR FLOW SILTSACK®
(FOR AREAS OF LOW TO MODERATE PRECIPITATION AND RUN-OFF)

PROPERTIES	TEST METHOD	UNITS
GRAB TENSILE STRENGTH	ASTM D-4632	300 LBS
GRAB TENSILE ELONGATION	ASTM D-4632	20 %
PUNCTURE	ASTM D-4633	120 LBS
MULLEN BURST	ASTM D-3786	800 PSI
TRAPEZOID TEAR	ASTM D-4535	120 LBS
UV RESISTANCE	ASTM D-4355	90 %
APPARENT OPENING SIZE	ASTM D-4751	40 US SEIVE
FLOW RATE	ASTM D-4491	40 GAL/MIN/SQ FT
PERMITTIVITY	ASTM D-4491	0.35 SEC -1

HI-FLOW SILTSACK®
(FOR AREAS OF MODERATE TO HEAVY PRECIPITATION AND RUN-OFF)

PROPERTIES	TEST METHOD	UNITS
GRAB TENSILE STRENGTH	ASTM D-4632	265 LBS
GRAB TENSILE ELONGATION	ASTM D-4632	20 %
PUNCTURE	ASTM D-4633	135 LBS
MULLEN BURST	ASTM D-3786	420 PSI
TRAPEZOID TEAR	ASTM D-4535	45 LBS
UV RESISTANCE	ASTM D-4355	90 %
APPARENT OPENING SIZE	ASTM D-4751	20 US SEIVE
FLOW RATE	ASTM D-4491	200 GAL/MIN/SQ FT
PERMITTIVITY	ASTM D-4491	1.5 SEC -1

OIL-ABSORBANT SILTSACK®
(FOR AREAS WHERE THERE IS A CONCERN FOR OIL RUN-OFF OR SPILLS)

DEPENDENT ON YOUR PARTICULAR APPLICATION, THE SILTSACK® CAN BE MADE FROM EITHER ONE OF THE ABOVE FABRICS WITH AN OIL-ABSORBANT PILLOW INSERT OR, MADE COMPLETELY FROM AN OIL-ABSORBANT SILTSACK®, WITH A WOVEN PILLOW INSERT.

SITE PLAN REVIEW

MEDWAY PLANNING & ECONOMIC DEVELOPMENT BOARD

DATE: _____

CERTIFICATE OF VOTE

SEE CERTIFICATE OF VOTE FOR SITE PLAN APPROVAL DATED, MAY, 2018 FOR PLAN ENTITLED, '9 TROTTER DRIVE, A SITE PLAN IN MEDWAY MASSACHUSETTS, FROM THE MEDWAY PLANNING BOARD, VOTED TO APPROVE WITH STANDARD CONDITIONS.

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ENGINEERING DESIGN CONSULTANTS, INC.'S LIABILITY FOR THIS PLAN IS LIMITED TO THE EXTENT OF ITS FEE LESS THIRD PARTY COSTS.

DRWN BY: ZRS
CHKD BY: PSB
APPROV BY: VML

REVISIONS	DATE	DESCRIPTION
1	6/12/18	PEER REVIEW COMMENTS
2	3/26/18	SITE PLAN REVIEW
3	3/26/18	DESCRIPTION

WALTER M. LEWINSKY
CIVIL
No. 32327
REGISTERED PROFESSIONAL ENGINEER

PREPARED BY: *Walter M. Lewinsky*

Engineering Design Consultants, Inc.
32 Turnpike Road
Southborough, Massachusetts
ph:(508) 480-0225 fax:(508)832-5781

SITE PLAN
9 TROTTER DRIVE
MEDWAY, MASSACHUSETTS 02053

DETAILS
SITE PLAN

OWNER: MARGUERITE K. MELE
203 MAIN STREET
MEDWAY, MASSACHUSETTS 02053

APPLICANT: CTS PROPERTY MANAGEMENT
430 FRANKLIN VILLAGE DRIVE - SUITE 177
FRANKLIN, MASSACHUSETTS 02038

FILE NO: 3583
DATE: MARCH 26, 2018
PLAN NUMBER: 6 of 6

ENGINEERING DESIGN CONSULTANTS, INC. SHALL NOT BE RESPONSIBLE FOR CONSTRUCTION MEANS, METHODS, TECHNIQUES, OR PROCEDURES UTILIZED BY THE CONTRACTOR, NOR FOR THE SAFETY OF PUBLIC OR CONTRACTOR EMPLOYEES, OR FOR THE FAILURE OF THE CONTRACTOR TO CARRY OUT THE WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.

ENGINEERING DESIGN CONSULTANTS, INC.'S LIABILITY FOR THIS PLAN IS LIMITED TO THE EXTENT OF ITS FEE LESS THIRD PARTY COSTS.

DESIGNED BY: ZSB
CHECKED BY: PSB
APPROVED BY: WML

REVISIONS:
1 6/12/18
2 6/26/18

PEER REVIEW COMMENTS
DATE: 6/26/18
DESCRIPTION: SITE PLAN REVIEW



PREPARED BY:
Engineering Design Consultants, Inc.
32 Turnpike Road
Southborough, Massachusetts
ph: (508) 480-0225 fax: (508) 832-5781



SITE PLAN
9 TROTTER DRIVE
MEDWAY, MASSACHUSETTS 02053

PROJECT:

TITLE:

STORMWATER POLLUTION PREVENTION PLAN

OWNER:

MARGUERITE K. MELE
203 MAIN STREET
MEDWAY, MASSACHUSETTS 02053

APPLICANT:

CTS PROPERTY MANAGEMENT
430 FRANKLIN VILLAGE DRIVE - SUITE 177
FRANKLIN, MASSACHUSETTS 02038

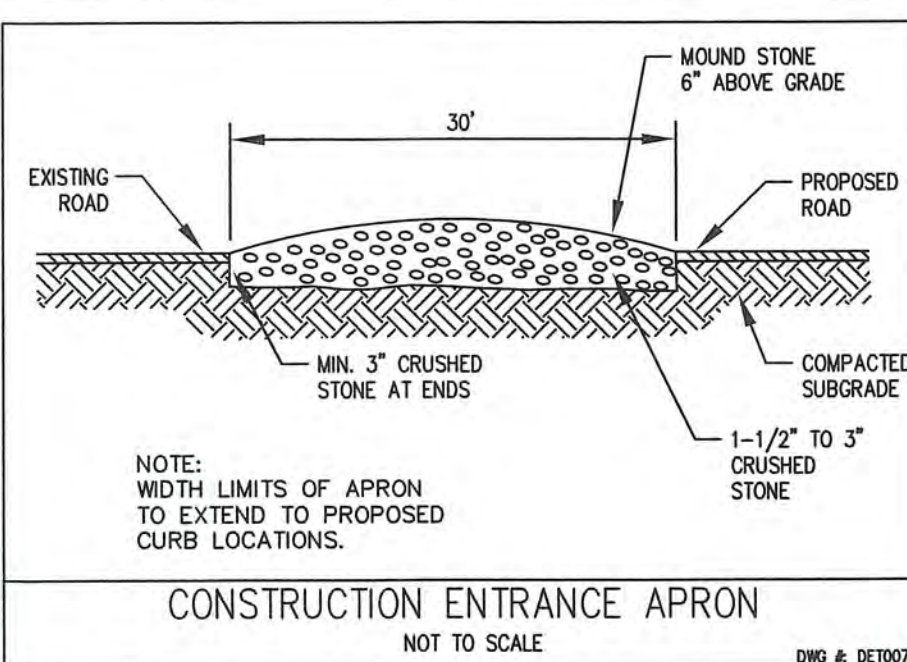
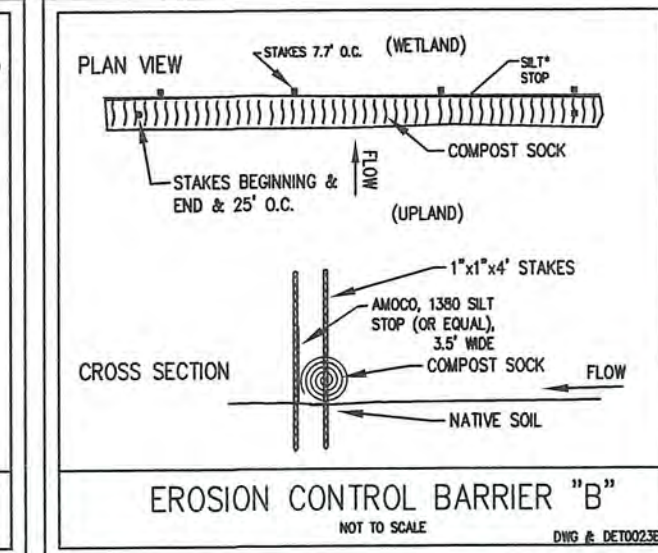
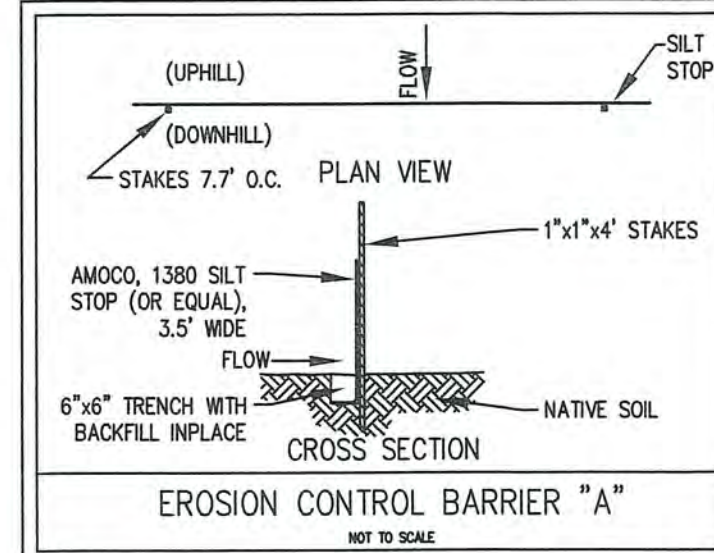
FILE NO.: 3583
STORMWATER POLLUTION PREVENTION PLAN
DATE: MARCH 26, 2018
DEFINITIVE PLAN NO.: 1 of 1

TROTTER DRIVE - OPERATION AND MAINTENANCE PLAN:

- SCHEDULE OF SHORT-TERM OPERATION AND MAINTENANCE (DURING CONSTRUCTION):
 - EROSION CONTROL BARRIER: THE EROSION CONTROL BARRIER SHALL BE INSTALLED PRIOR TO COMMENCEMENT OF CONSTRUCTION AND SHALL BE INSPECTED WEEKLY, PRIOR TO STORM EVENTS AND IMMEDIATELY AFTER STORM EVENTS TO ENSURE ITS INTEGRITY. PORTIONS OF THE EROSION CONTROL BARRIER SHALL BE REMOVED AS NECESSARY TO PREVENT EROSION.
 - CONSTRUCTION COMPLETION: THE ENTIRE STORM WATER MANAGEMENT SYSTEM SHALL BE INSPECTED UPON COMPLETION OF CONSTRUCTION. PORTIONS OF THE SYSTEM CONTAINING SEDIMENTATION SHALL BE PROPERLY CLEANED AND THE SILT REMOVED.
- MAINTENANCE AGREEMENT/SCHEDULE FOR LONG-TERM OPERATION AND MAINTENANCE (POST CONSTRUCTION)
 - OWNER OF THE STORMWATER SYSTEM: _____ DATE: _____
OWNERS SIGNATURE: _____
 - PERSON/ENTITY RESPONSIBLE FOR FINANCING MAINTENANCE AND EMERGENCY REPAIRS: _____
 - VORTSENTRY UNIT INSPECTIONS: INSPECTION IS THE KEY TO EFFECTIVE MAINTENANCE AND IS EASILY PERFORMED. POLLUTANT DEPOSITION AND TRANSPORT MAY VARY FROM YEAR TO YEAR, AND REGULAR INSPECTIONS WILL HELP INSURE THAT THE SYSTEM IS CLEANED OUT AT THE APPROPRIATE TIME.
AT A MINIMUM, INSPECTIONS SHOULD BE PERFORMED FOUR TIMES PER YEAR, HOWEVER MORE OR LESS FREQUENT INSPECTIONS MAY BE NECESSARY WHERE FREQUENT WINTER SANDING OPERATIONS MAY LEAD TO RAPID ACCUMULATIONS. THE VISUAL INSPECTION SHOULD ASCERTAIN THAT THE SYSTEM COMPONENTS ARE IN WORKING ORDER AND THAT THERE ARE NO BLOCKAGES OR OBSTRUCTIONS TO INLET AND/OR SEPARATION SCREEN. THE INSPECTION SHOULD ALSO IDENTIFY ACCUMULATIONS OF HYDROCARBONS, TRASH, AND SEDIMENT IN THE SYSTEM. MEASURING POLLUTANT ACCUMULATION CAN BE DONE WITH A CALIBRATED DIPSTICK, TAPE MEASURE OR OTHER MEASURING INSTRUMENT. IT IS USEFUL, AND OFTEN REQUIRED AS PART OF A PERMIT TO KEEP A RECORD OF EACH INSPECTION. (SEE THE INSPECTION AND MAINTENANCE GUIDE PROVIDED BY THE MANUFACTURER)
 - CLEANING OF THE VORTSENTRY SYSTEM SHOULD BE DONE WHEN SEDIMENT ACCUMULATES AT A DEPTH OF TWO FEET WITHIN THE UNIT AND SHOULD BE DONE DURING DRY WEATHER CONDITIONS WHEN NO FLOW IS ENTERING THE SYSTEM. CLEANOUT OF THE CDS WITH A VACUUM TRUCK IS GENERALLY THE MOST EFFECTIVE AND CONVENIENT METHOD OF EXCAVATING POLLUTANTS FROM THE SYSTEM. SIMPLY REMOVE THE MANHOLE COVERS AND INSERT THE VACUUM HOSE INTO THE SUMP. THE SYSTEM SHOULD BE COMPLETELY DRAINED DOWN AND THE SUMP FULLY EVACUATED OF SEDIMENT. THE AREA OUTSIDE THE UNIT SHOULD BE PUMPED OUT ALSO IF POLLUTANT BUILD-UP EXISTS IN THIS AREA.
 - BUILDING GUTTERS: THE PROPOSED BUILDING WILL CONTAIN GUTTERS. ALL STORMWATER FROM ROOF RUNOFF IS DIRECTED TOWARD INFILTRATION SYSTEM LOCATED AT THE REAR OF THE BUILDING. THE DOWNSPOUTS WILL BE DIRECTED TO THE INFILTRATION TRENCHES THROUGH ROOF LEADER PIPES. GUTTERS SHALL BE CLEANED PERIODICALLY AS NEEDED AND/OR AT LEAST TWICE PER YEAR, AFTER LEAVES HAVE FALLEN IN THE FALL SEASON AND IN THE SPRING SEASON AFTER NEW LEAF GROWTH. IT MAY BE DETERMINED UPON INSPECTION THAT CLEANING MORE OFTEN MAY BE NECESSARY. DOWNSPOUT STRAINERS SHALL BE USED AT DOWNSPOUT LOCATIONS WITHIN THE TRENCHES.

NOTE: UNDERGROUND UTILITY LOCATIONS ARE NOT GUARANTEED.

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION.
DIGSAFE IS TO BE NOTIFIED 72 BUSINESS HOURS IN ADVANCE OF CONSTRUCTION. CALL DIGSAFE AT 811



FLOW DIFFUSER

NOT TO SCALE

- EROSION CONTROL MEASURES (SEE DETAILS) SHALL BE INSTALLED PRIOR TO COMMENCEMENT OF CONSTRUCTION AS INDICATED.
- MEASURES SHALL BE TAKEN TO LIMIT THE TRACKING OF MATERIAL FROM THE SITE ONTO RUSTIC ROAD AND SUMMER STREET.
- DUST SHALL BE CONTROLLED BY SPRINKLING OR OTHER APPROVED MEASURES AS NECESSARY.
- THROUGHOUT CONSTRUCTION, ALL DISTURBED AREAS SHALL BE LOAMED AND SEEDED AND MULCHED AS SOON AS PRACTICABLE IN ORDER TO AVOID OVERLAND TRANSPORT OF SEDIMENT.
- EROSION CONTROL MEASURES ARE TO BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD AND PARTICULARLY AFTER SIGNIFICANT RAINFALL. DILAPIDATED HAIRBALES AND SILT STOP SHALL BE REPLACED AS REQUIRED TO MAINTAIN THE INTEGRITY OF THE BARRIER.
- WITH SLOPES STABILIZED AND FINAL INSPECTION COMPLETE, THE EROSION CONTROL BARRIER SHALL BE REMOVED AND PROPERLY DISPOSED OFF-SITE.

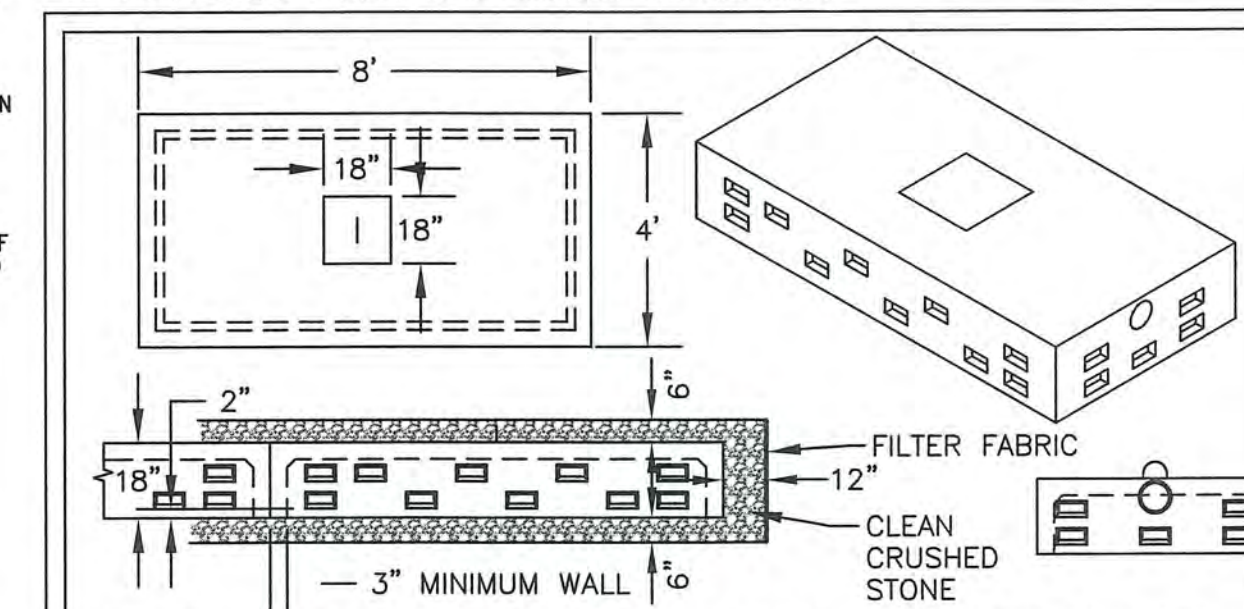
SEDIMENTATION & EROSION CONTROL METHODS & PROCEDURES

DETAIL: EC0002

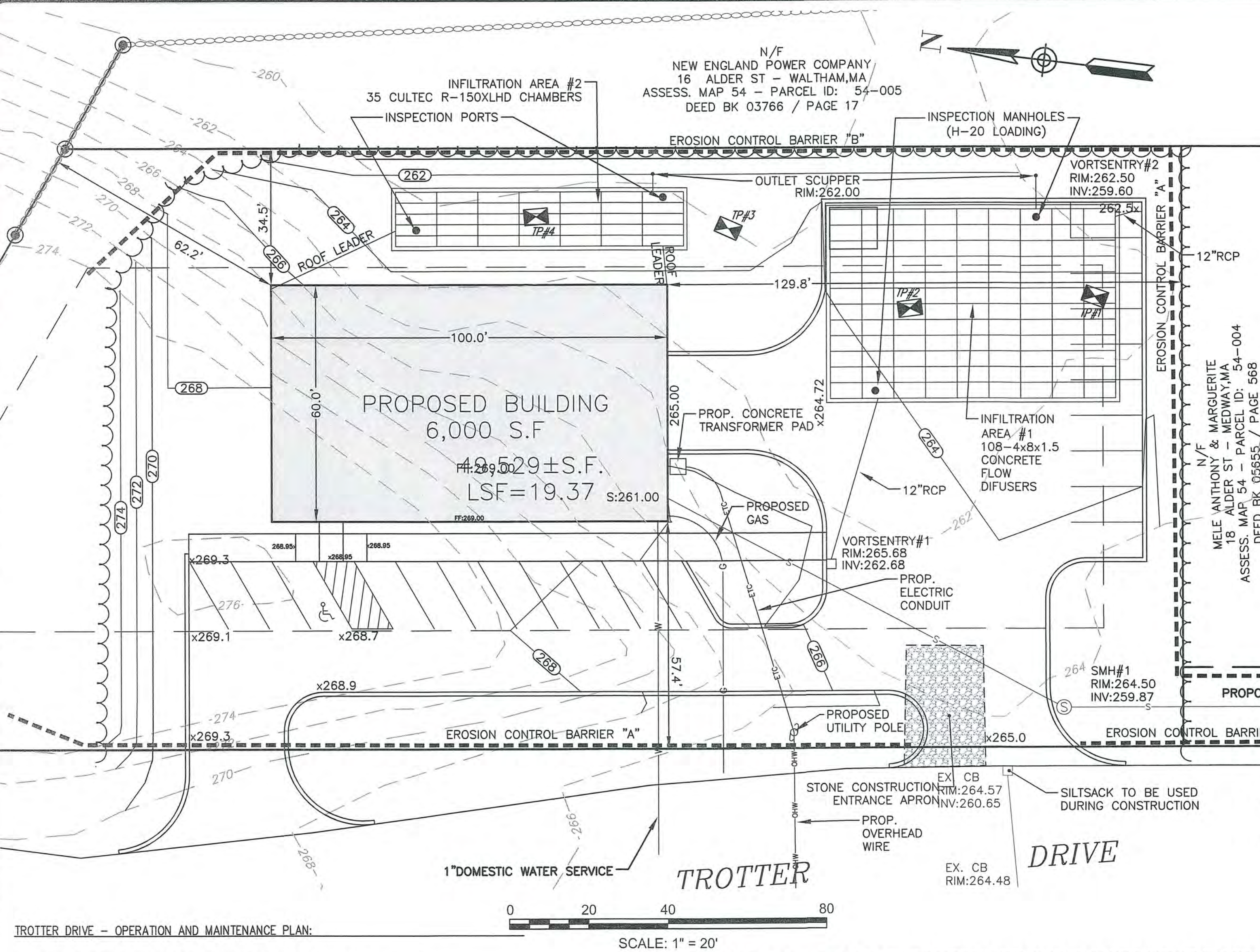
CONSTRUCTION NOTES

- EROSION CONTROL GRASS MIXTURE (SIDE SLOPE GREATER THAN 4 HORIZONTAL TO 1 VERTICAL) FOLLOWING COMPLETION OF GRADING. THE FOLLOWING SEED MIX SHALL BE APPLIED:

TYPE OF SEED	% BY WEIGHT
CREeping RED FESCUE	30
TALL FESCUE	30
WHITE DUTCH CLOVER	30
RED TOP	10
- SEED AT THE RATE OF 2#/2,000 SF
USE THE ABOVE MIX AT THE RATE OF 50#/ACRE AND WITH 1/2 BUSH/ACRE OF WINTER RYE
- ALL DISTURBED AREAS ARE TO BE LOAMED AND SEEDED WITH A MINIMUM OF 6" OF TOP SOIL SPREAD EVENLY THROUGHOUT. PROVIDE EROSION CONTROL MEASURES AS NECESSARY TO PROVIDE SLOPE STABILITY UNTIL VEGETATION IS ESTABLISHED.

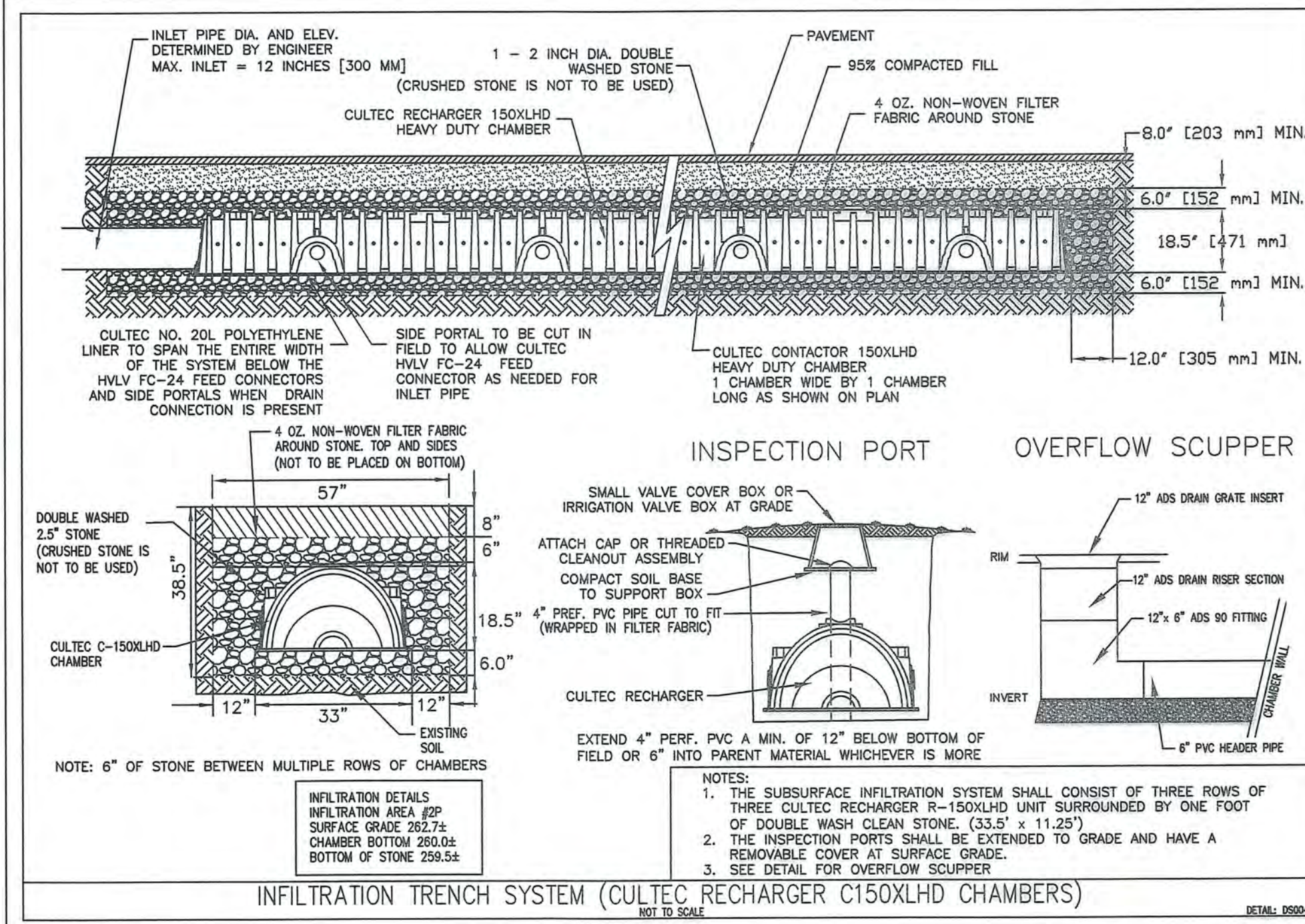
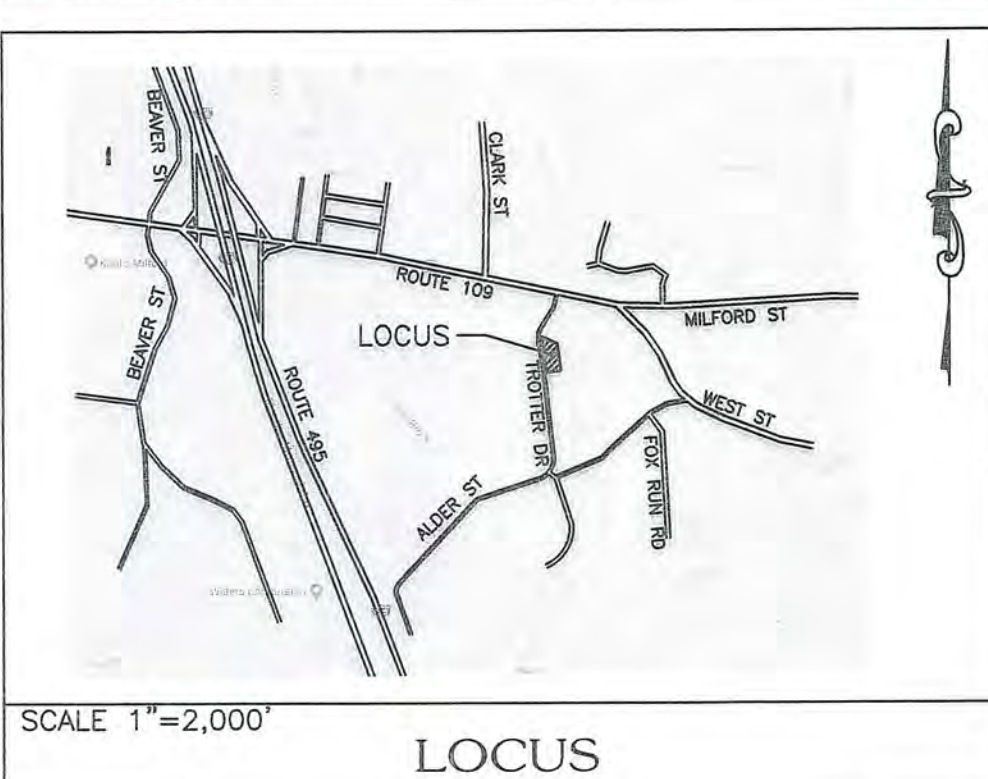


THE FLOW DIFFUSERS SHALL BE PLACED ON 6" OF CLEAN CRUSHED STONE AND THE ENTIRE INFILTRATION SYSTEM SHALL BE SURROUNDED BY 1-FOOT OF CLEAN CRUSHED STONE AND A 4 OZ. NON-WOVEN FILTER FABRIC..



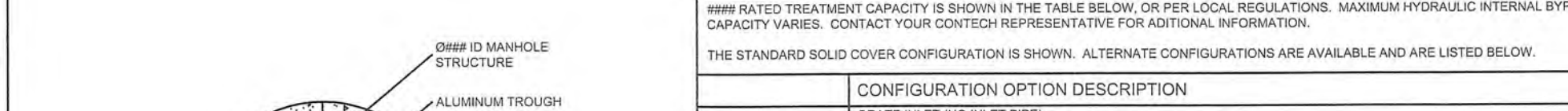
- NAME OF PERSON AND ENTITY RESPONSIBLE FOR PLAN COMPLIANCE: _____
- PROJECT MANAGER OF 15 TROTTER DRIVE CONSTRUCTION IS THE CONTACT PERSON. CONTACT NUMBER IS _____
- CONSTRUCTION PERIOD POLLUTION PREVENTION MEASURES: AS DEFINED WITHIN.
- EROSION AND SEDIMENTATION CONTROL PLAN DRAWINGS: REFER TO PLAN VIEW FOR EROSION CONTROL MEASURE LOCATIONS.
- DETAIL DRAWINGS AND SPECIFICATIONS FOR EROSION CONTROL: REFER TO INDIVIDUAL DETAILS FOR INSTALLATION OF EROSION CONTROL MEASURES.
- VEGETATION PLANNING: THE PROPOSED CONSTRUCTION ACTIVITIES WILL BE PERFORMED WITHIN THE LIMITS OF WORK AREA (SILTATION BARRIER BEING THE LIMIT OF WORK). THE FINAL PROPOSED SURFACE WILL CONSIST OF PAVING AND GRASSED AREAS (REFER TO SEQUENCING BELOW).
- SITE DEVELOPMENT PLAN: REFER TO PLAN VIEW & APPROVED SITE PLANS.
- CONSTRUCTION SEQUENCING PLAN: THE FOLLOWING CONSTRUCTION SEQUENCE SHALL BE FOLLOWED:
 - EROSION CONTROL PROVISIONS IN PLACE AND INSPECTED PRIOR TO ANY AND ALL CONSTRUCTION.
 - ANTI-TRACKING STONE CONSTRUCTION ENTRANCE.
 - CLEAR AND GRUB.
 - STRIP AND STOCKPILE TOPSOIL. PROVIDE TEMPORARY SEEDING OF STOCKPILE, PLACE SILTATION BARRIER AROUND STOCKPILE AND PLACE TARPS OVER THE STOCKPILE IF NECESSARY.
 - ROUGH GRADE PROPOSED SITE.
 - EXCAVATE AND CONSTRUCT UTILITIES SYSTEMS. REMOVE UNUSABLE MATERIAL FROM SITE.
 - INSTALL WATER PIPE, DRAINAGE, VORTSENTRY UNITS, AND OTHER UTILITIES.
 - EROSION CONTROL MEASURES AT NEW DRAINAGE INLETS.
 - PLACE BASE PAVING.
 - CONSTRUCT BUILDING.
 - FINAL GRADING.
 - LOAM AND SEED ALL DISTURBED AREAS AND LANDSCAPE YARDS.
 - FINAL TOP COURSE PAVING.
 - REMOVE EROSION CONTROL PROVISIONS UPON STABILIZATION AND FINAL SITE INSPECTION BY APPROVING AUTHORITY.
- SEQUENCING OF EROSION AND SEDIMENTATION CONTROLS: EROSION CONTROL PROVISIONS IN PLACE AND INSPECTED PRIOR TO ANY AND ALL CONSTRUCTION.
- OPERATION AND MAINTENANCE OF EROSION AND SEDIMENTATION CONTROLS: THE CONTRACTOR/OPERATOR IS RESPONSIBLE FOR MAINTAINING A STABLE SITE. THE OPERATOR SHALL EMPLOY EROSION AND SEDIMENTATION CONTROLS TO PREVENT EROSION AND SEDIMENT RELEASES BEYOND THE SEDIMENT BARRIER. THE EROSION CONTROL MEASURES INDICATED ARE THE MINIMUM THAT SHOULD BE EMPLOYED. THE CONTRACTOR/OPERATOR SHALL EMPLOY ADDITIONAL CONTROLS AS THE ON-SITE CONSTRUCTION EXPERIENCE DICTATES.
- INSPECTION SCHEDULE:
 - INSPECTIONS SHALL TAKE PLACE AT LEAST EVERY 7 CALENDAR DAYS OR WITHIN 24 HOURS OF THE END OF A STORM EVENT OF 0.25" OF RAIN OR GREATER.
 - INSPECTION FREQUENCY MAY BE REDUCED TO ONCE A MONTH IF THE ENTIRE SITE IS TEMPORARILY STABILIZED OR RUNOFF IS UNLIKELY DUE TO WINTER CONDITIONS OR EXTREMELY DRY CONDITIONS.
 - INSPECTIONS MUST BE CONDUCTED BY A PROPERLY AUTHORIZED QUALIFIED PERSONNEL.
 - INSPECTIONS SHALL INCLUDE ALL AREAS OF THE SITE DISTURBED BY CONSTRUCTION ACTIVITIES. INSPECTIONS MUST LOOK FOR EVIDENCE OF POLLUTANTS OR POTENTIAL POLLUTANTS ENTERING THE STORM WATER SYSTEM. SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSPECTED TO ENSURE PROPER OPERATION. LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE MUST BE INSPECTED FOR EVIDENCE OF OFF-SITE SEDIMENT TRACKING.

- MAINTENANCE SCHEDULE: REPORTS OF SUMMARIZING THE INSPECTIONS SHOULD BE KEPT AS PART OF THIS CONSTRUCTION PERIOD POLLUTION PREVENTION AND SOIL EROSION AND SEDIMENTATION CONTROL PLAN. AT A MINIMUM THE INSPECTION REPORTS SHALL INCLUDE:
 - INSPECTION DATE;
 - NAME, TITLE AND QUALIFICATIONS OF THE PERSONNEL MAKING THE INSPECTION;
 - WEATHER CONDITIONS AT THE TIME OF INSPECTION AND SINCE THE PREVIOUS INSPECTION, INCLUDING A BEST ESTIMATE OF THE BEGINNING OF EACH STORM EVENT THAT OCCURRED, DURATION OF EACH STORM EVENT, APPROXIMATE AMOUNT OF RAINFALL FOR EACH STORM EVENT, AND WHETHER ANY DISCHARGES HAVE OCCURRED;
 - DISCHARGES THAT OCCUR AT THE TIME OF INSPECTION;
 - LOCATION OF DISCHARGES OR SEDIMENTATION FROM THE SITE;
 - LOCATION OF "BMPs" THAT NEED TO BE MAINTAINED;
 - LOCATION OF "BMPs" THAT FAILED TO OPERATE AS DESIGNED OR PROVED INADEQUATE FOR A PARTICULAR LOCATION;
 - LOCATIONS WHERE ADDITIONAL "BMPs" ARE NEEDED THAT DID NOT EXIST AT THE TIME OF INSPECTION;
 - CORRECTIVE ACTION REQUIRED INCLUDING ANY CHANGES TO THIS PLAN.
 - ALL REPORTS SHALL BE SUBMITTED TO THE DEPARTMENT OF PUBLIC WORKS SUPERINTENDENT ON A WHEN REQUESTED DURING THE CONSTRUCTION PERIOD, AND MAINTAINED FOR THREE YEARS AFTER FINAL CONSTRUCTION PROJECT CLOSEOUT IS COMPLETE.



INFILTRATION TRENCH SYSTEM (CULTEC RECHARGER C150XLHD CHAMBERS)

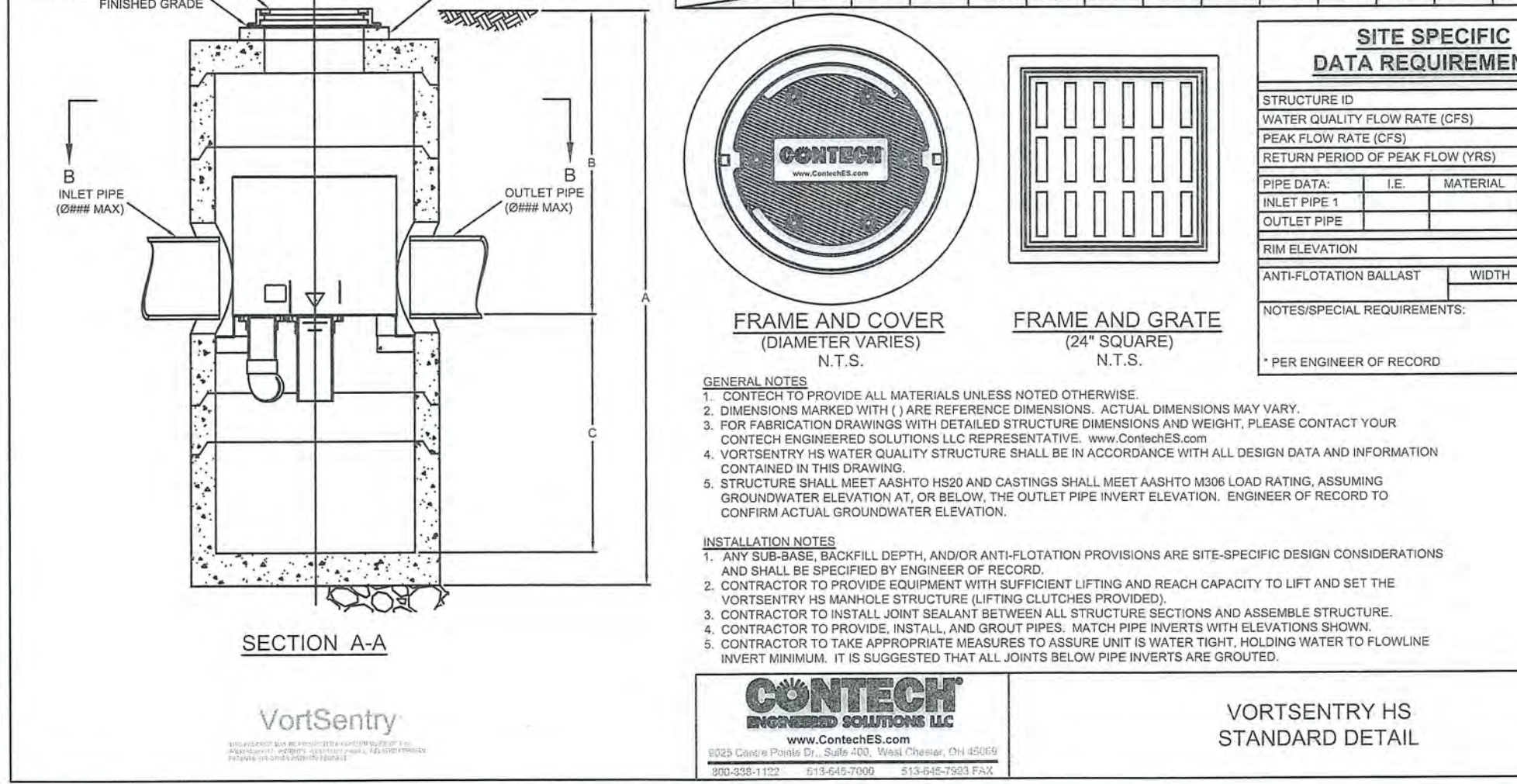
NOT TO SCALE



INSPECTION PORT OVERFLOW SCUPPER

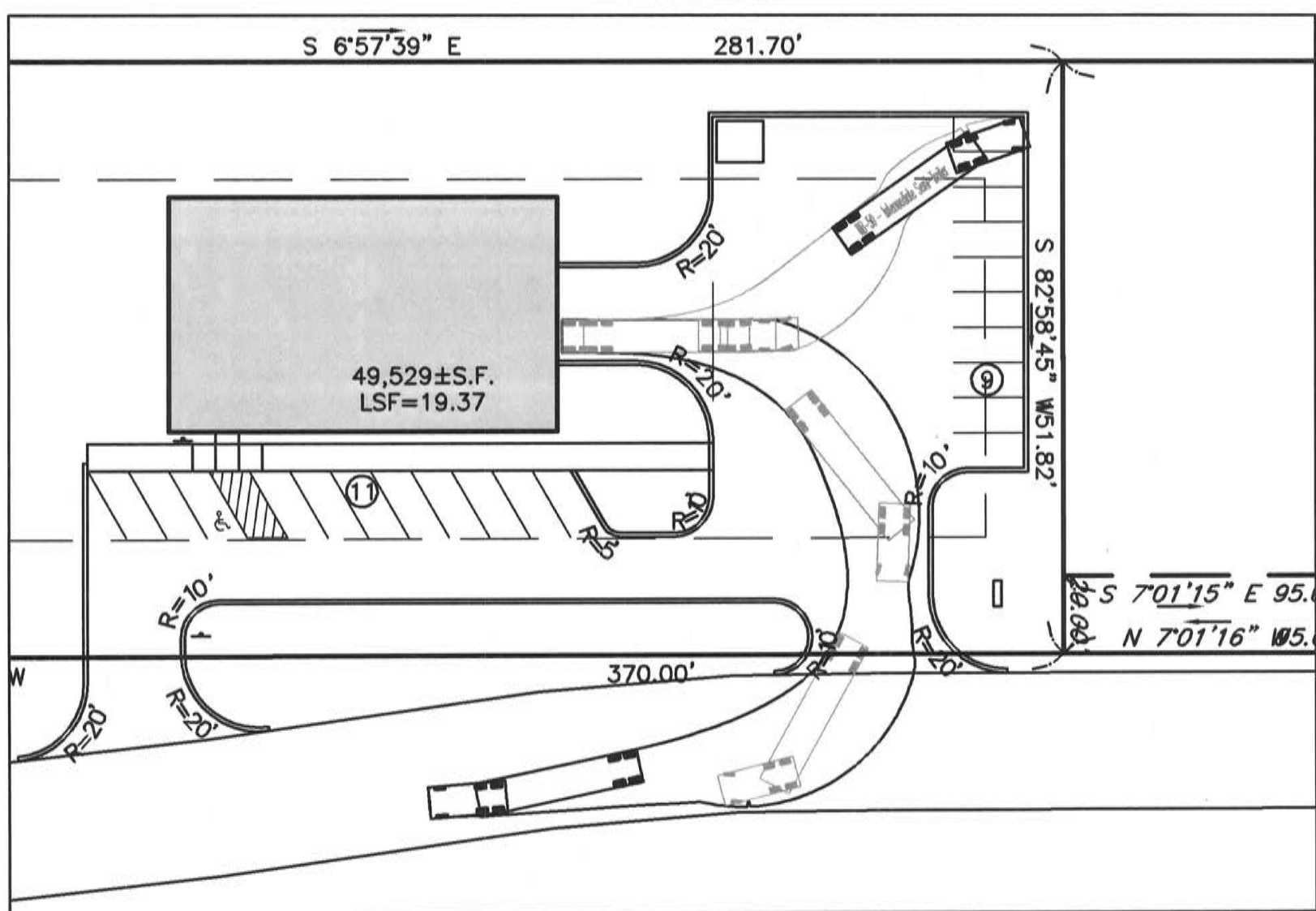
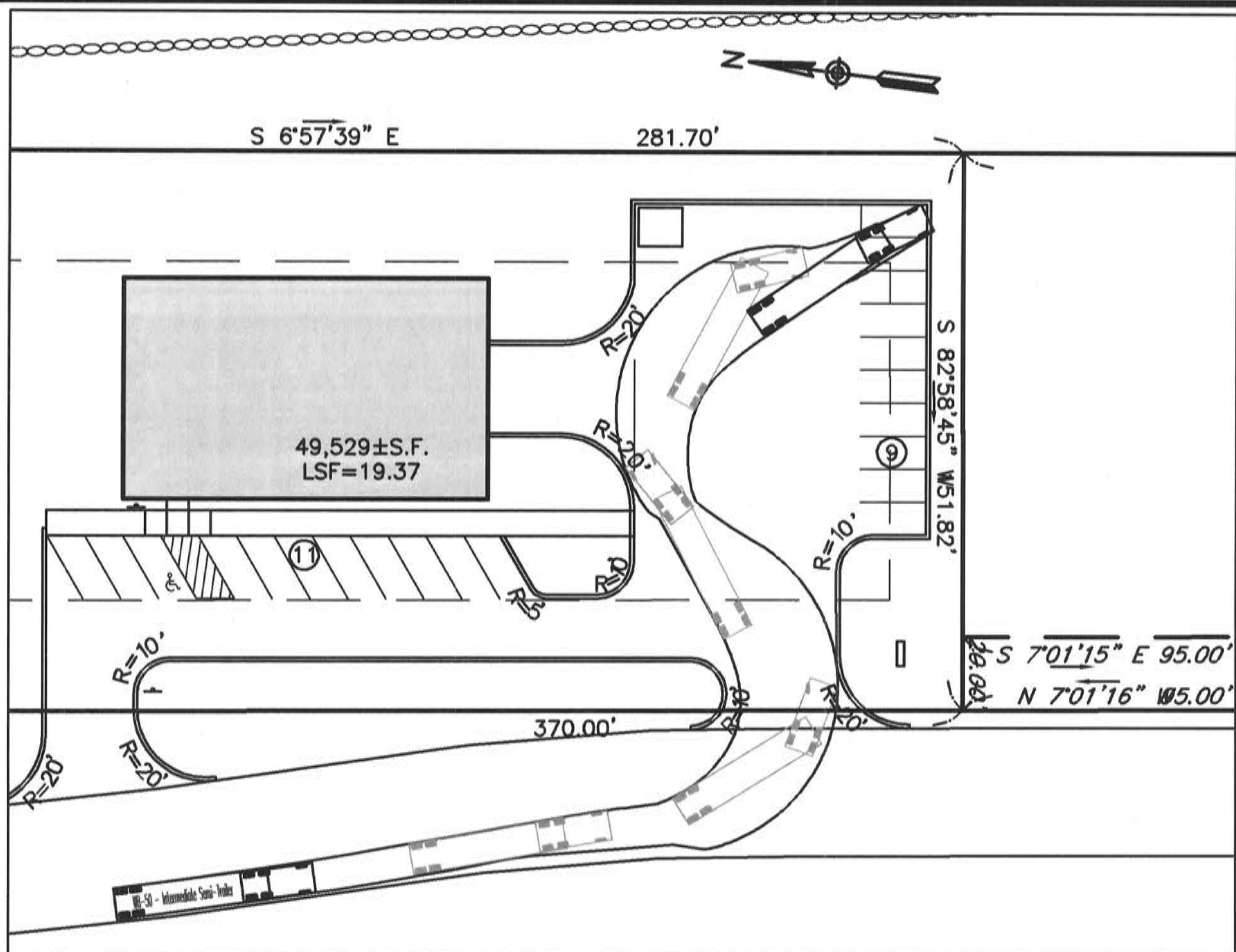
NOT TO SCALE

VORTSENTRY HS DESIGN NOTES									
MINIMUM RATED TREATMENT CAPACITY IS SHOWN IN THE TABLE BELOW, OR PER LOCAL REGULATIONS. MAXIMUM HYDRAULIC INTERNAL BYPASS CAPACITY VARIES. CONTACT YOUR COUNTER REPRESENTATIVE FOR ADDITIONAL INFORMATION.									
THE STANDARD SOLID COVER CONFIGURATION IS SHOWN. ALTERNATE CONFIGURATIONS ARE AVAILABLE AND ARE LISTED BELOW.									
CONFIGURATION OPTION DESCRIPTION									
GRATE INLET WITH INLET PIPE									
VORTSENTRY HS GENERAL INFORMATION									
Model	Manhole Diameter (ID)	Total Treatment Flow Rate	Typical Total Distance Rim to Outside Bottom	Typical Distance Rim to Invert	Typical Depth Below Invert (inches)	Approximate Minimum Distance Rim to Invert	Maximum Pipe Diameter (ID)		
H535	3	900	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	4	1800	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	5	2700	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	6	3600	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	7	4500	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	8	5400	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	9	6300	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	10	7200	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	11	8100	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	12	9000	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	13	9900	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	14	10800	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	15	11700	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	16	12600	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	17	13500	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	18	14400	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	19	15300	0.55	15.6	10.16	3.10	4.08	1.24	5.58
H535	20	16200	0.55	15.6	10.16	3.10	4.08	1.24	5.58



VORTSENTRY HS STANDARD DETAIL

NOT TO SCALE



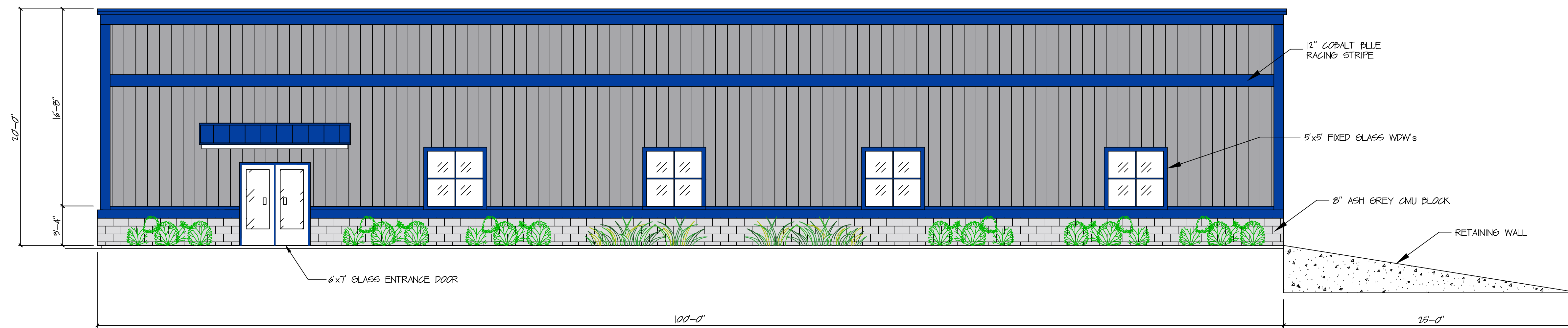
PREPARED BY:



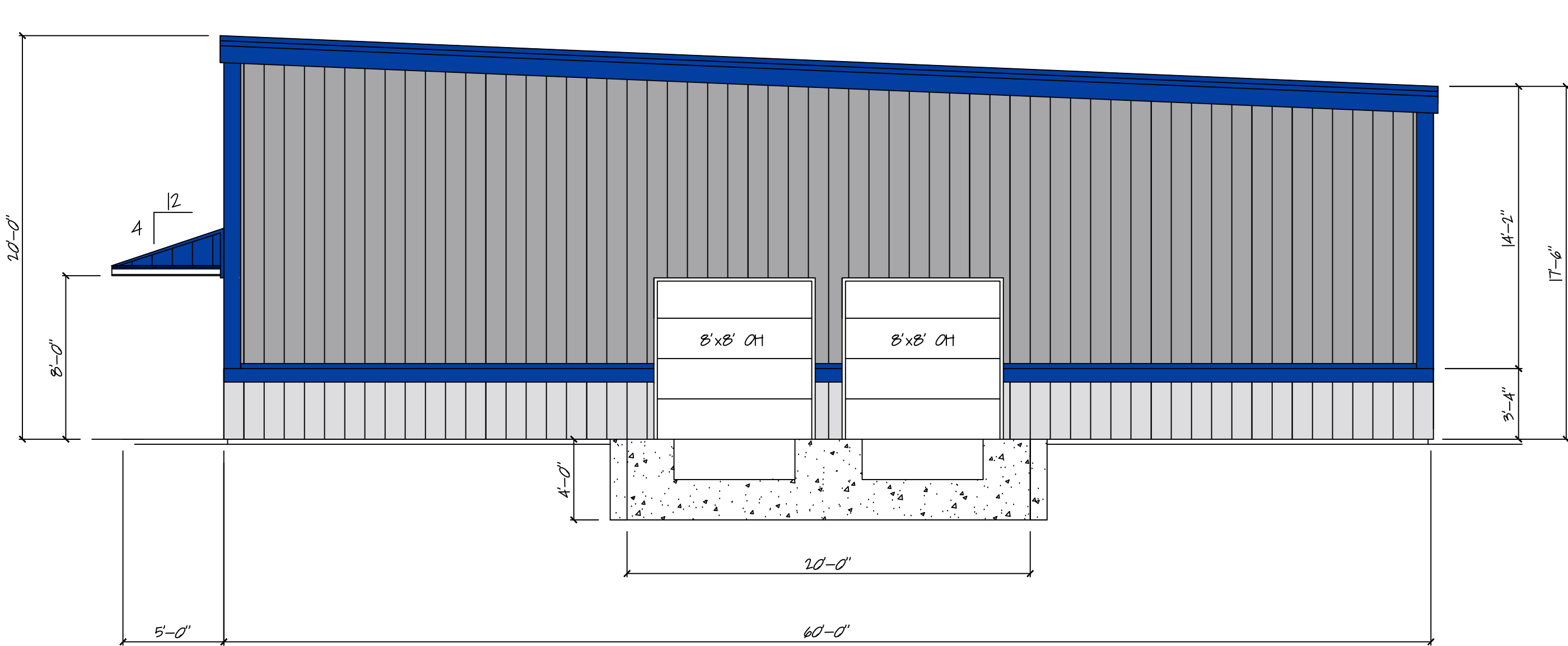
Engineering Design Consultants, Inc.
32 Turnpike Road
Southborough, Massachusetts
ph:(508) 480-0225 fax:(800)832-5781

TRUCK TURNING MOVEMENTS
15 TROTTER DRIVE
MEDWAY, MASSACHUSETTS 02053

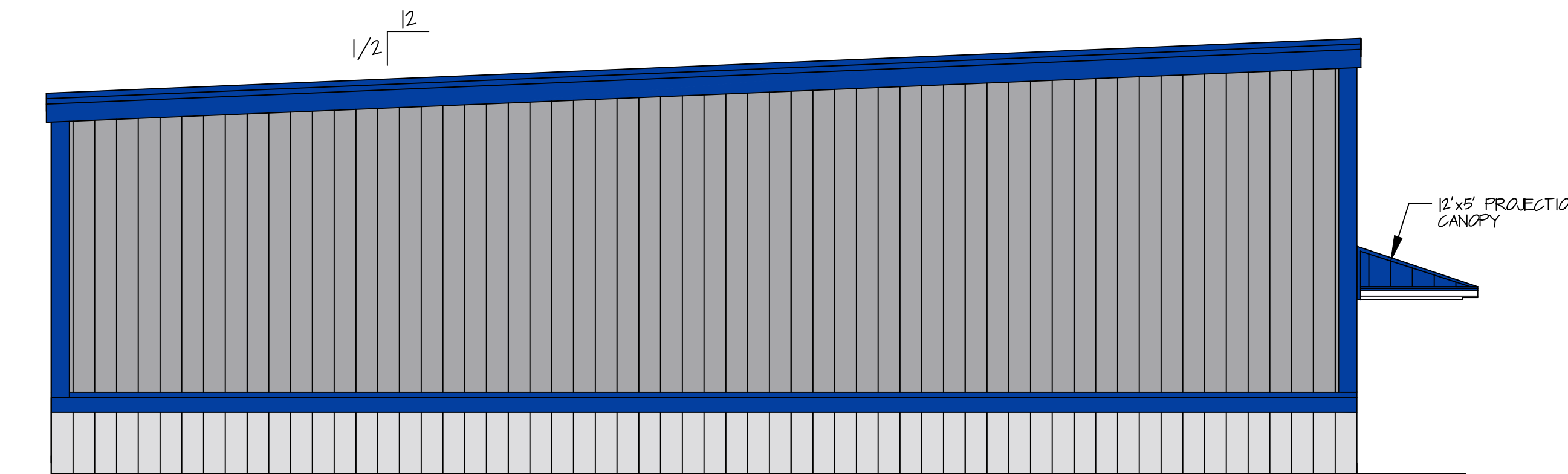
JUNE 12, 2018



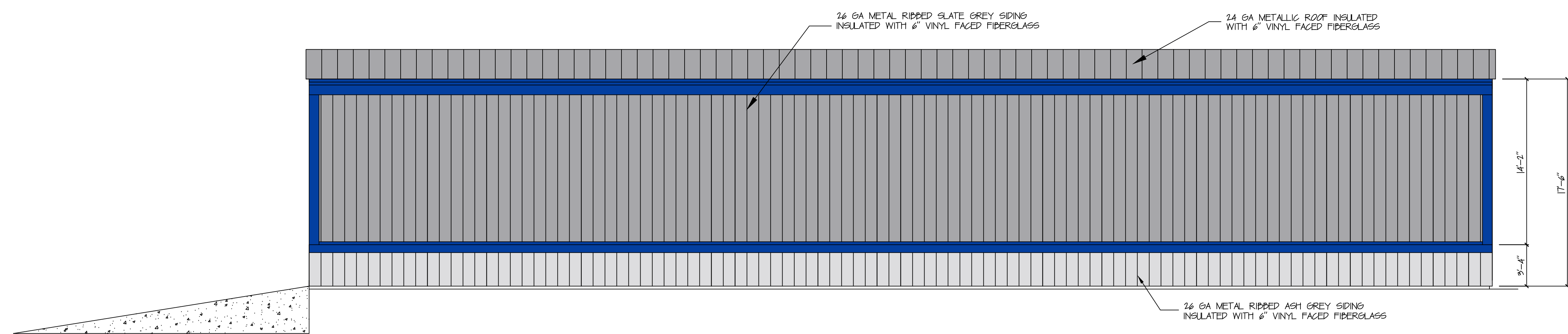
FRONT ELEVATION



RIGHT ELEVATION



LEFT ELEVATION



REAR ELEVATION

General Notes

TED GREENLAW
183 COLUMBIA STREET
HANOVER, MA


No.	Revision/Issue	Date

Firm Name and Address
PRIGGEN STEEL
133 FRANKLIN STREET
WRENTHAM, MA


Project Name and Address
CTS
430 FRANKLIN VILLAGE DR
FRANKLIN, MA

Project ELEVATION	Sheet S-2
Date 6/20/18	
Scale 3/16" = 1'-0"	

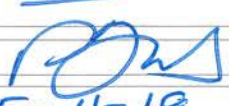
Medway Planning and Economic Development Board
Request for Waiver from Site Plan Rules and Regulations
 Complete 1 form for each waiver request

Project Name:	9 Trotter Drive
Property Location:	9 Trotter Drive, Medway, MA
Type of Project/Permit:	Major Site Plan
Identify the number and title of the relevant Section of the Site Plan Rules and Regulations from which a waiver is sought.	204-A Standards for Site Plan Preparation
Summarize the text of the relevant Section of the Rules and Regulations from which a waiver is requested.	B. The site plan shall be drawn at a scale of 1"=40'
What aspect of the Regulation do you propose be waived?	1"=40'
What do you propose instead?	1"=20' + 1"=10'
Explanation/justification for the waiver request. Why is the waiver needed? Describe the extenuating circumstances that necessitate the waiver request.	The small size of the project requires a larger scale to show the detail of the design.
What is the estimated value/cost savings to the applicant if the waiver is granted?	NO SAVINGS
How would approval of this waiver request result in a superior design or provide a clear and significant improvement to the quality of this development?	BETTER CLARITY OF PROJECT INFO.
What is the impact on the development if this waiver is denied?	NO IMPACT
What are the design alternatives to granting this waiver?	PLOT DRAWINGS AT 40 SCALE
Why is granting this waiver in the Town's best interest?	BETTER CLARITY OF PROJECT INFO.
If this waiver is granted, what is the estimated cost savings and/or cost avoidance to the Town?	NO COST
What mitigation measures do you propose to offset not complying with the particular Rule/Regulation?	NO MITIGATION
What is the estimated value of the proposed mitigation measures?	NO VALUE
Other Information?	
Waiver Request Prepared By:	
Date:	5-11-18
Questions?? - Please contact the Medway PED office at 508-533-3291.	


Medway Planning and Economic Development Board
Request for Waiver from Site Plan Rules and Regulations
 Complete 1 form for each waiver request

Project Name:	9 Trotter Drive
Property Location:	9 Trotter Drive, Medway, MA
Type of Project/Permit:	Major Site Plan
Identify the number and title of the relevant Section of the Site Plan Rules and Regulations from which a waiver is sought.	204-5 Site Plan Contents
Summarize the text of the relevant Section of the Rules and Regulations from which a waiver is requested.	C.3. Existing Landscape Inventory
What aspect of the Regulation do you propose be waived?	EXISTING LANDSCAPE INVENTORY
What do you propose instead?	EXISTING CONDITIONS AS PRESENTED
Explanation/justification for the waiver request. Why is the waiver needed? Describe the extenuating circumstances that necessitate the waiver request.	SITE PARTIALLY WOODED WITH NO SIGNIFICANT OR REDEEMING VEGETATIONS NEEDING IDENTIFICATION OR PROTECTION
What is the estimated value/cost savings to the applicant if the waiver is granted?	NOMINAL VALUE
How would approval of this waiver request result in a superior design or provide a clear and significant improvement to the quality of this development?	TREE SALE RETAINED AT WORK LIMITS
What is the impact on the development if this waiver is denied?	NO CHANGE
What are the design alternatives to granting this waiver?	NO CHANGE
Why is granting this waiver in the Town's best interest?	NO NEGATIVE IMPACT FROM WAIVER
If this waiver is granted, what is the estimated cost savings and/or cost avoidance to the Town?	NO COST
What mitigation measures do you propose to offset not complying with the particular Rule/Regulation?	SUPPLEMENTAL PLANTINGS PROVIDED AT STREET & ALONG WORK LIMIT
What is the estimated value of the proposed mitigation measures?	\$15K
Other Information?	
Waiver Request Prepared By:	
Date:	5-11-19
Questions?? - Please contact the Medway PED office at 508-533-3291.	


Medway Planning and Economic Development Board
Request for Waiver from Site Plan Rules and Regulations
 Complete 1 form for each waiver request

Project Name:	9 Trotter Drive
Property Location:	9 Trotter Drive, Medway, MA
Type of Project/Permit:	Major Site Plan
Identify the number and title of the relevant Section of the Site Plan Rules and Regulations from which a waiver is sought.	205-6 Parking
Summarize the text of the relevant Section of the Rules and Regulations from which a waiver is requested.	G. Parking Spaces/Stalls
What aspect of the Regulation do you propose be waived?	3a) Stalls shall be 10' x 20'
What do you propose instead?	9' x 18' + Angle stalls
Explanation/justification for the waiver request. Why is the waiver needed? Describe the extenuating circumstances that necessitate the waiver request.	CREATING ONE-WAY TRAVEL FLOW PARKING SPACES ARE SUFFICIENTLY SIZED TO ACCOMMODATE DEVELOPMENT
What is the estimated value/cost savings to the applicant if the waiver is granted?	NOMINAL COST
How would approval of this waiver request result in a superior design or provide a clear and significant improvement to the quality of this development?	LESS ASPHALT REQUIRED
What is the impact on the development if this waiver is denied?	ADDITIONAL ASPHALT
What are the design alternatives to granting this waiver?	RECONFIGURATION OF PARKING FIELD
Why is granting this waiver in the Town's best interest?	EQUAL DESIGN WITH LESS IMPERVIOUS AREA
If this waiver is granted, what is the estimated cost savings and/or cost avoidance to the Town?	NOMINAL SAVINGS
What mitigation measures do you propose to offset not complying with the particular Rule/Regulation?	ADDED OPEN LAND
What is the estimated value of the proposed mitigation measures?	NOMINAL VALUE
Other Information?	
Waiver Request Prepared By:	
Date:	5-11-18
Questions?? - Please contact the Medway PED office at 508-533-3291.	


Medway Planning and Economic Development Board
Request for Waiver from Site Plan Rules and Regulations
 Complete 1 form for each waiver request

Project Name:	9 Trotter Drive
Property Location:	9 Trotter Drive, Medway, MA
Type of Project/Permit:	Major Site Plan
Identify the number and title of the relevant Section of the Site Plan Rules and Regulations from which a waiver is sought.	205-6 Parking
Summarize the text of the relevant Section of the Rules and Regulations from which a waiver is requested.	G. Parking Spaces/stalls
What aspect of the Regulation do you propose be waived?	3b. Wheel stops
What do you propose instead?	Cape Cod Berms + Integral Concrete Walk + Curb
Explanation/justification for the waiver request. Why is the waiver needed? Describe the extenuating circumstances that necessitate the waiver request.	WHEEL STOPS IMPOSE OBSTRUCTIONS WITHIN PARKING FIELD
What is the estimated value/cost savings to the applicant if the waiver is granted?	\$2500
How would approval of this waiver request result in a superior design or provide a clear and significant improvement to the quality of this development?	ELIMINATES OBSTRUCTIONS, PROVIDES FOR BETTER SNOW REMOVAL AND SITE MANAGEMENT
What is the impact on the development if this waiver is denied?	LESS DESIRABLE SITE CONDITION
What are the design alternatives to granting this waiver?	NONE
Why is granting this waiver in the Town's best interest?	NO ADVERSE IMPACT TO TOWN
If this waiver is granted, what is the estimated cost savings and/or cost avoidance to the Town?	NONE
What mitigation measures do you propose to offset not complying with the particular Rule/Regulation?	NONE
What is the estimated value of the proposed mitigation measures?	NONE
Other Information?	
Waiver Request Prepared By:	
Date:	5-11-19
Questions?? - Please contact the Medway PED office at 508-533-3291.	

Medway Planning and Economic Development Board
Request for Waiver from Site Plan Rules and Regulations
 Complete 1 form for each waiver request

Project Name:	9 Trotter Drive
Property Location:	9 Trotter Drive, Medway, MA
Type of Project/Permit:	Major Site Plan
Identify the number and title of the relevant Section of the Site Plan Rules and Regulations from which a waiver is sought.	205-6 Parking G. Parking Spaces/Stalls
Summarize the text of the relevant Section of the Rules and Regulations from which a waiver is requested.	4b. The stalls shall not be located within 15' of the front, side & rear property lines
What aspect of the Regulation do you propose be waived?	
What do you propose instead?	10' Min. to the side property line.
Explanation/justification for the waiver request. Why is the waiver needed? Describe the extenuating circumstances that necessitate the waiver request.	LOADING ZONE LIMITS FOR TRUCK TRAVEL TO AVOID PARKED VEHICLES. SCREENING PROVIDED ALONG LOTLINE
What is the estimated value/cost savings to the applicant if the waiver is granted?	NOMINAL COST
How would approval of this waiver request result in a superior design or provide a clear and significant improvement to the quality of this development?	ADD MORE IMPERVIOUS AREA
What is the impact on the development if this waiver is denied?	INCREASE ASPHALT WITHIN LOADING AREA
What are the design alternatives to granting this waiver?	" " " " "
Why is granting this waiver in the Town's best interest?	REDUCE IMPERVIOUS LIMITS
If this waiver is granted, what is the estimated cost savings and/or cost avoidance to the Town?	NO COST
What mitigation measures do you propose to offset not complying with the particular Rule/Regulation?	SCREENING ALONG LOT LINE
What is the estimated value of the proposed mitigation measures?	\$7K
Other Information?	
Waiver Request Prepared By:	
Date:	5-11-18
Questions?? - Please contact the Medway PED office at 508-533-3291.	

Medway Planning and Economic Development Board
Request for Waiver from Site Plan Rules and Regulations
 Complete 1 form for each waiver request

Project Name:	9 Trotter Drive
Property Location:	9 Trotter Drive, Medway, MA
Type of Project/Permit:	Major Site Plan
Identify the number and title of the relevant Section of the Site Plan Rules and Regulations from which a waiver is sought.	204-3 Planning Board Submittals
Summarize the text of the relevant Section of the Rules and Regulations from which a waiver is requested.	A.7. A written Development Impact Statement.
What aspect of the Regulation do you propose be waived?	ALL SECTIONS EXCEPT TRAFFIC
What do you propose instead?	ADDRESS TRAFFIC WITH NARRATIVE
Explanation/justification for the waiver request. Why is the waiver needed? Describe the extenuating circumstances that necessitate the waiver request.	PROJECT SCALE + SCOPE IS SMALL BEING ONLY 6,000 SF ON ABOUT 1 AC. TRAFFIC IS ADDRESSED.
What is the estimated value/cost savings to the applicant if the waiver is granted?	\$2500
How would approval of this waiver request result in a superior design or provide a clear and significant improvement to the quality of this development?	NO ADVERSE IMPACTS FROM NOT ADDRESSING REMAINING SECTIONS OF A.7
What is the impact on the development if this waiver is denied?	NO IMPACT TO PROJECT
What are the design alternatives to granting this waiver?	DESIGN WILL NOT CHANGE AS A RESULT
Why is granting this waiver in the Town's best interest?	PROJECT HAS MINIMAL IMPACT
If this waiver is granted, what is the estimated cost savings and/or cost avoidance to the Town?	NO COST
What mitigation measures do you propose to offset not complying with the particular Rule/Regulation?	DESIGN UNCHANGED
What is the estimated value of the proposed mitigation measures?	NO VALUE
Other Information?	
Waiver Request Prepared By:	
Date:	5-11-18
Questions?? - Please contact the Medway PED office at 508-533-3291.	



June 26, 2018
Medway Planning & Economic Development Board
Meeting

Construction Reports

- Emails dated June 6, 15 and 22 from Steve Bouley/Tetra Tech with mini inspection reports for Applegate subdivision
- Beals and Thomas report dated June 15, 2018 for Exelon project

Susan Affleck-Childs

From: Bouley, Steven <Steven.Bouley@tetrattech.com>
Sent: Wednesday, June 20, 2018 1:11 PM
To: Susan Affleck-Childs
Subject: Applegate Erosion Control Inspection #1

Hi Susy,

I visited the site on Wednesday, June 6, 2018 and had the following comments:

1. Roadway has sediment tracking from construction at the individual house lots mainly from Lots 7B, 9B, and 10B. roadway should be swept and construction entrances maintained as necessary to prevent further tracking of sediment into the roadway.
2. The silt sacks in the catch basins at the low point of the roadway remain in a state where they need to be cleaned, there is also sediment built up down there that needs to be swept. Runoff is ponding at these cb's because it cannot get through the silt sack. However, it does not appear sediment has traveled downstream to the basin. When the applicant is changing the silt sacks they should measure the amount of sediment in the structures, if it is equal to half of the sump depth it should be cleaned per the O&M plan. I can't remember if these structures were ever cleaned, it may be a good time for the applicant to inspect, and if required, clean all structures per the O&M plan since the drainage system is operational.
3. There is a tree that has fallen across the basin outlet structure, it should be removed as well as woody vegetation that is growing in the rip-rap at the basin inlet point. Mowing should continue regularly throughout the growing season to prevent woody vegetation from establishing within the basin limits.

Please let me know if you need anything else, thanks.

Steve

Steven M. Bouley, P.E. | Senior Project Engineer
Direct: 508.786.2382 | Main: 508.786.2200 | Fax: 508.786.2201
steven.bouley@tetrattech.com

Tetra Tech, Inc. | United States Infrastructure (USI) Division
Marlborough Technology Park | 100 Nickerson Road, Suite 200 | Marlborough, MA 01752 www.tetrattech.com

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Susan Affleck-Childs

From: Bouley, Steven <Steven.Bouley@tetrattech.com>
Sent: Friday, June 15, 2018 3:25 PM
To: Susan Affleck-Childs
Cc: Bridget Graziano; Andy Rodenhiser (andy@rodenhiser.com); Barbara Saint Andre; Jack Mee
Subject: Applegate Erosion Control Inspection #2
Attachments: Photos_2018-06-13.pdf

Hi Susy,

I went to the site Yesterday to check on the status of the erosion controls at the site and have the following comments and attached photos:

1. Much of the roadway has been cleaned up, particularly the areas adjacent to the entrances to the lots under construction, I didn't see any tracking on the pavement from those sites.
2. The silt sacks in the catch basins at the low point of the roadway remain in a state where they need to be cleaned, there is also sediment built up down there that needs to be swept.
3. Stockpiles of material on Lot 5B and 6B are unprotected and will require either covering or placing erosion controls around to prevent migration in the event of a storm.
4. The infiltration basin requires mowing and removal of the tree that has fallen adjacent to the outlet control structure. Vegetation should also be removed from the rip-rap at the inlet to the basin as well as the rip-rap cleaned. It does not appear groundwater is discharging to the basin any longer.

Please let me know if you need anything else, thanks.

Steve

Steven M. Bouley, P.E. | Senior Project Engineer
Direct: 508.786.2382 | Main: 508.786.2200 | Fax: 508.786.2201
steven.bouley@tetrattech.com

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PLEASE DO NOT
DUMP NO WASTE DRAINS TO WATER

TO WATERWAYS
INDIA









Susan Affleck-Childs

From: Bouley, Steven <Steven.Bouley@tetrattech.com>
Sent: Thursday, June 21, 2018 9:15 PM
To: Susan Affleck-Childs
Subject: Applegate Erosion Control Inspection #3

Hi Susy,

I visited the site yesterday and have the following comments:

1. The silt sacks in the catch basins at the low point of the roadway remain in a state where they need to be cleaned, there is also sediment built up down there that needs to be swept.
2. Stockpiles of material on Lot 5B and 6B are unprotected and will require either covering or placing erosion controls around to prevent migration in the event of a storm.
3. The infiltration basin requires mowing and removal of the tree that has fallen adjacent to the outlet control structure. Vegetation should also be removed from the rip-rap at the inlet to the basin as well as the rip-rap cleaned. It does not appear groundwater is discharging to the basin any longer.

Please let me know if you need anything else, thanks.

Steve

Steven M. Bouley, P.E. | Senior Project Engineer
Direct: 508.786.2382 | Main: 508.786.2200 | Fax: 508.786.2201
steven.bouley@tetrattech.com

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BEALS + THOMAS

BEALS AND THOMAS, INC.
Reservoir Corporate Center
144 Turnpike Road
Southborough, MA 01772-210



ExelonGeneration®

PROGRESS INSPECTION REPORT

Inspection Date: 6/15/2018

Inspector: Eric J. Las, PE, LEED AP, Beals and Thomas, Inc.

Inspection Report Number: 10

PERMIT COMPLIANCE

Proceeding per approved site plan?

YES ☒ NO ☐ If not, note area and explain:

This report has been prepared in compliance with Specific Condition IX.M.4 of the Site Plan Decision, dated July 26, 2016

Project Name:

West Medway II

Location: Medway, MA

B+T Job#: 1422.10

Local Approvals: Order of Conditions DEP File No. 216-0879, Site Plan Decision, Host Community Agreement

Introduction:

Exelon West Medway II LLC respectfully submits this construction progress inspection report for the West Medway II project. The purpose of this report is to document the in-progress work with respect to the approved site plans and associated conditions for the Planning and Economic Development Board. This report summarizes noteworthy activities conducted on site from May 15, 2018 to June 15, 2018. Please also refer to the enclosed Photo Log.

For limited areas of the site not observed by Beals and Thomas, items denoted herein with an () are based on Exelon's Erosion Control Inspection Reports.

Current Work Activities, Comments, and Observations:

- Finished grading of the central portion of the site infrastructure is nearing completion.
- The demineralized water tank and raw water tank installations are complete. Tanks await completion of painting and testing.
- Fuel oil tank installation is complete. Hydrostatic testing is in progress. Once the test is complete, water will be pumped into the demineralized and raw water tanks.
- Installation of the earthen containment berm around fuel oil tank is in progress.
- Installation of fuel piping throughout the central portion of the site is in progress.
- Excavation and shaping of the rain garden is in progress.
- Concrete pours have diminished with only a few small miscellaneous pours remaining.
- Installation of the acoustic barriers is in progress in the central portion of the site.
- Installation of the ammonia tank enclosure is in progress.
- Interior finish work continues on the Admin Building.
- Various components of the power generating system are now set on foundations and are being plumbed and wired.
- Electric utility installation at the Metering & Regulating Station will be completed in June to connect to the adjacent existing transformer.
- Installation of the security fence around the Metering & Regulating Station is in progress.
- Various soil stockpiles are actively stabilized via tarps and seeding where necessary.
- *Silt fences, straw bales, and straw wattles are being routinely monitored and maintained as needed.
- *Silt sacks in the catch basins in Summer Street are being routinely monitored and maintained as needed.
- *24/7 security details remain in place at site entrance.


Authorized Signature

6/15/18

Date

PROPERTY OWNER:

Exelon West Medway II, LLC

Attn: Todd Cutler, Esq.
Associate General Counsel

Phone: 610-765-5602

Email: todd.cutler@exeloncorp.com

Attn: Pete Callahan,
Project Director

Phone: 617-381-2332

Email: Pete.Callahan@constellation.com

Attn: Doug Blakeley,
Environmental Monitor

Phone: 518-265-7354

Email: doug.blakeley@aptim.com

TOWN OF MEDWAY

Attn: Michael E. Boynton,
Town Administrator

Phone: 508-533-3264

Email: mboynton@townofmedway.org

Attn: Bridget Graziano,
Conservation Agent

Phone: 508-533-3292

Email: bgraziano@townofmedway.org

Attn: Susan Affleck-Childs, Planning &
Economic Development Coordinator

Phone: 508-533-3291

Email: sachilds@townofmedway.org

ENVIRONMENTAL CONSULTANTS

Beals and Thomas, Inc.

Attn: Eric J. Las, PE, LEED AP
Principal

Phone: 508-366-0560

Email: elas@bealsandthomas.com

Epsilon Associates, Inc.

Attn: Michael Howard
Principal & Manager

Phone: 978-461-6247

Email: mhoward@epsilonassociates.com

**BEALS + THOMAS****Exelon Generation**

PHOTOGRAPHIC LOG

Client Name:Exelon West
Medway II**Photo:** West Medway II**Location:** Medway, MA**Project No:**

1422.10

**Photo
No: 1****Date:**
6/20/18**Description:**

Aerial view facing west.

Work continues in the central portion of the site. Acoustic walls, overhead wires, and rain garden installation is in progress.

**Client Name:**Exelon West
Medway II**Photo:** West Medway II**Location:** Medway, MA**Project No:**



1422.10



**Photo
No: 2****Date:**
6/20/18**Description:**



Aerial view facing northeast.

Shaping of Infiltration Basin-01 is in progress. Heat exchanger installation is complete. Water tanks await hydrostratic testing.



Client Name: Exelon West Medway II		Photo: West Medway II Location: Medway, MA	Project No: 1422.10
Photo No: 3	Date: 6/15/18		
Description: View facing northeast. Fuel oil tank installation is complete. Installation of earthen containment berm around tank is in progress.			
Client Name: Exelon West Medway II		Photo: West Medway II Location: Medway, MA	Project No: 1422.10
Photo No: 4	Date: 6/15/18		
Description: View facing south. Installation of fuel piping is in progress throughout the central portion of the site.			

Client Name: Exelon West Medway II		Photo: West Medway II Location: Medway, MA	Project No: 1422.10
Photo No: 5	Date: 5/15/18		
Description: View facing north. Installation of the acoustic walls around the energy generating components is in progress in the central portion of the site.			
Client Name: Exelon West Medway II		Photo: West Medway II Location: Medway, MA	Project No: 1422.10
Photo No: 6	Date: 6/15/18		
Description: View facing west. Installation of the enclosure for the ammonia tank is in progress.			

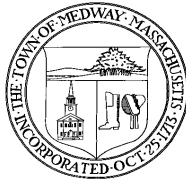
Client Name: Exelon West Medway II		Photo: West Medway II Location: Medway, MA	Project No: 1422.10
Photo No: 7	Date: 5/15/18		
Description: View facing north. Excavation and shaping of the rain garden is in progress in the central portion of the site.			
Client Name: Exelon West Medway II		Photo: West Medway II Location: Medway, MA	Project No: 1422.10
Photo No: 8	Date: 6/15/18		
Description: View facing east. Installation of the security fence for the Metering and Regulating Station is in progress in the northern portion of the site.			



June 26, 2018
Medway Planning & Economic Development Board
Meeting

Committee Representative and Liaison
Appointments

- Memo from Susy Affleck-Childs dated June 7, 2018



TOWN OF MEDWAY
Planning & Economic Development
155 Village Street
Medway, Massachusetts 02053

June 7, 2018

TO: Planning & Economic Development Board Members
FROM: Susy Affleck-Childs
RE: Board Reorganization - Board/Committee Representatives and Liaisons

It is that time of year for the PEDB to elect its officers and to select PEDB members to serve on or function as liaisons to other Town boards and committees for fiscal year 19 (July 1, 2018 – June 30, 2019). Please review the list below and be prepared to discuss your interests at the 6/26/2018 PEDB meeting.

Presently Assigned

FY 19

***The PEDB has official representation on the following committees.
This involves regular attendance at meetings.***

Community Preservation Committee	Matt Hayes	_____
Design Review Committee	Tom Gay	_____
Street Naming Committee	Susy Affleck-Childs	_____
Economic Development Committee	Rich Di Iulio	_____
Energy Committee	Bob Tucker	_____
Open Space & Rec. Plan Update Task Force	Matt Hayes	_____
Sign Bylaw Review Task Force	Tom Gay	_____
Oak Grove Zoning Task Force	Matt Hayes	_____
EPFRAC (Evaluation of Parks, Fields and Recreation Areas Committee)	None	_____

Board/Committee Liaisons

Affordable Housing Committee & Trust	Andy Rodenhiser	_____
Agricultural Commission	Bob Tucker	_____
Board of Assessors	Andy Rodenhiser	_____
Board of Health	Andy Rodenhiser	_____
Board of Water/Sewer Commissioners	Andy Rodenhiser	_____
Capital Improvements Planning	Matt Hayes	_____
Conservation Commission	Bob Tucker	_____

508-533-3291

planningboard@townofmedway.org

Finance Committee	Bob Tucker	_____
Historical Commission	Matt Hayes	_____
Open Space Committee	Rich Di Iulio	_____
Redevelopment Authority	Andy Rodenhiser	_____
Route 109 Reconstruction	Matt Hayes	_____
SWAP	Rich Di Iulio	_____
Town Administrator/Board of Selectmen	Andy Rodenhiser	_____
Zoning Board of Appeals	Bob Tucker	_____

Other Groups

Medway Business Council	Andy Rodenhiser	_____
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June 26, 2018
Medway Planning & Economic Development Board
Meeting

The Haven Subdivision (Sorrento Lane)
Lot Release
UPDATED

The developer, Ray Doiran, expects to convey the first house in early July and has requested that the Board sign the standard document to release the lots from the Subdivision Covenant. This is normally something that a buyer's attorney and lender require to have in hand before closing.

They have completed the minimum required subdivision infrastructure work for you to do so.

Attached is the lot release document for your review and signature.

LAND SUBDIVISION

Release of Restrictive Covenant

Planning & Economic Development Board – Town of Medway, MA

We, the undersigned members, being a majority of the Planning & Economic Development Board of the Town of Medway, Norfolk County, Massachusetts, hereby certify on this date that Lot 1, 2, 3 on a plan entitled "The Haven, A Private Way Definitive Subdivision Plan" in Medway, MA, with a final revision date of October 10, 2015, prepared by MERRIKIN ENGINEERING, LLP CONSULTING ENGINEERS 730 MAIN STREET, SUITE 2C MILLIS, MA 02054, which is recorded with the Norfolk County Registry of Deeds in Plan Book 655, Page 46, to which reference may be had for a more particular description, is hereby released from the terms, provisions and conditions as to sale and building thereon as set forth in a Covenant between the Medway Planning and Economic Development Board and Louis J. Cheschi, Jr., as Trustee of L & L Realty Trust u/d/t dated September 23, 1997, as identified by the Certificate of Trust recorded herewith dated November 8, 2016 and recorded with the Norfolk County Registry of Deeds in Book 34948, Page 77.

Executed under seal this _____ day of _____, 2018.

Signatures of a majority of the members of the Planning & Economic Development Board of the Town of Medway:

COMMONWEALTH OF MASSACHUSETTS

Norfolk County, SS. _____

On this _____ day of _____, 2018, before me, the undersigned notary public, personally appeared _____

_____,
members of the Medway Planning and Economic Development Board, proved to me through satisfactory evidence of identification, which was personal knowledge, to be the persons whose names are signed on the above document, and acknowledged to me that they signed it voluntarily and for its stated purpose.

Notary Public
My commission expires:



June 26, 2018
Medway Planning & Economic Development Board
Meeting

Zoning Work for Fall Town Meeting

- List of Staff Recommendations for Zoning Bylaw Amendments for November 2018 Town Meeting

Zoning Work for November 2018 Town Meeting

Kennel work - new definitions for various types of "kennels"; changes to Use Table; coordinate with Brenda Hamelin/ACO re: new state law	Mackenzie	
Interim Oak Grove Zoning to allow URP uses while the new zoning is being developed by OGZTF with Ted Brovitz; Discuss with MRA	Barbara	
Central Business District - Mixed Uses; replace existing Section 5.4.1	Mackenzie	
AUOD - Allow the specified AUOD uses by right if the property is already used for commercial purposes; require special permit for conversion of historic properties; remove requirement for residential component	Susy	
Incidental Accessory Items - Define; What can be located in what setbacks and what is completely prohibited in setbacks?	Jack	
Corner clearance distance - definition and requirement. Where does this belong in the ZBL?	Mackenzie	
Outside Storage - % of land or % of gross sq. ft. of structure; establish limits for the 4 industrial zones	Jack	
Revise 2 family /duplex definition; remove requirement in Use Table that a 2 family has to look like a single family house	Susy	
Modifications to multifamily overlay district provisions to protect historic resources, clarify exceptions, adjust minimum frontage and strengthen buffer language	Susy	
Miscellaneous housekeeping items	Barbara	

OTHER WORK FOR November TOWN MEETING

Land filling general bylaw	Bridget	
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Zoning Work for May 2019 Town Meeting

Use Variance - eliminate or limit		
West Street rezoning - new district?		
Certificate of Zoning Compliance		

Common Driveway - if, where to allow. Special Permit?		
Estate lot		
6/8/2018		



June 26, 2018
Medway Planning & Economic Development Board
Meeting

Correspondence

- Flyer re: June 25 OSRP Community Forum
- 495 Metro West Partnership Annual Report
- 495 Metro West Partnership regional demographic data report



Medway Community Forum

Open Space & Recreation Plan (OSRP)

Monday, June 25, 2018
7-9 PM (refreshments & socializing at 6:30 pm)

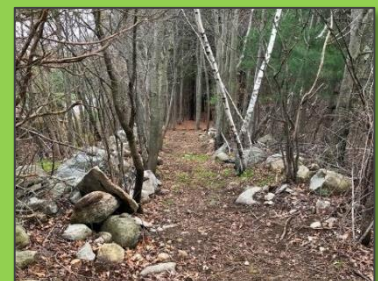
MEDWAY PUBLIC LIBRARY
Cole Room (Lower level meeting room)
26 High Street - Medway

You are invited to:

- Share your thoughts and ideas about land, open spaces, natural resources, and recreational facilities in Medway
- Discuss goals and priorities for Medway's new Open Space & Recreation Plan (OSRP)
- Review and provide comments on the Draft of the 7 Year OSRP Action Plan

What is an Open Space & Recreation Plan?

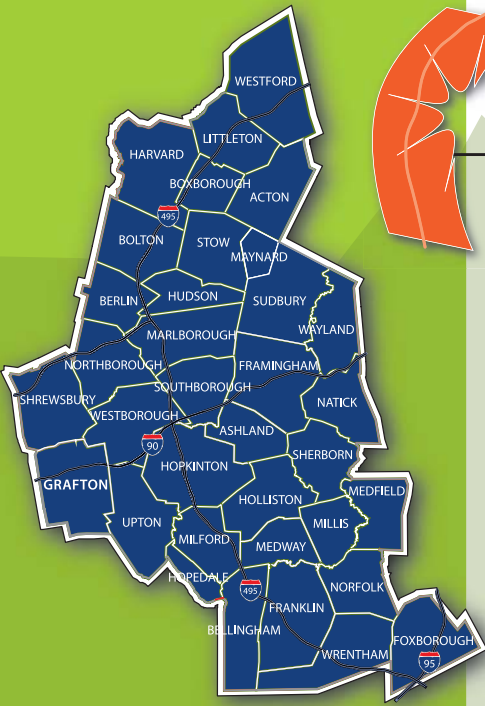
An Open Space and Recreation Plan (OSRP) is a tool to help communities inventory, maintain and enhance all the benefits of open space that contribute significantly to the character of the community. Open space includes water supplies, land, working farms and forests, wildlife habitat, parks, recreation areas, trails, greenways and conservation areas.



Sponsored by:
Town of Medway
Open Space and Recreation Plan Update Task Force
Community & Economic Development Department
508-533-3291
osrp@townofmedway.org

495/METROWEST PARTNERSHIP'S ANNUAL REPORT 2018-2019

PARTNERING FOR THE REGION, SUCCEEDING FOR THE COMMONWEALTH



"The 495/MetroWest Partnership is a key ally to our legislative delegation. Paul and Jessica are effective strategic partners in fighting toll impacts, bringing investment into the region including the impending upgrade to the I-495/90 interchange, and shaping our future. As members of the Partnership, municipalities, nonprofits, and businesses have a seat at the table in addressing some of the most important economic issues of our region."

—State Representative Carolyn Dykema

"The 495/MetroWest Partnership should be the first group a business engages with here in the Metro/West Region. The Partnership's focus on public private collaboration, programs and work to strengthen the region in all aspects is second to none. Sunovion has experienced firsthand the excellent work of the Partnership to collaborate with life sciences and highlight our industry in the region."

—Katrina Iserman, Director of State Government Affairs for Sunovion Pharmaceuticals, Inc.

THE 495/METROWEST PARTNERSHIP

was founded in 2003 to "promote economic vitality and sustain our natural resources while enhancing the quality of life in the 495/MetroWest region" and since then, has led efforts to focus attention on our region as an economic engine for the Commonwealth.

Thanks to the work of public and private leaders involved in the Partnership, policymakers now know that our regional economy is responsible for over \$23 Billion in annual payroll, is a net importer of labor, and hosts some of the state's top employers and critical industry clusters. With this success has come increased attention to our needs including highway interchanges and transit services, water supply and wastewater infrastructure, housing and commercial development, and the reuse of underutilized facilities. Beyond direct investments and policies benefiting our thirty-five communities, there is growing awareness that our region typifies the value of collaboration between the state, municipalities, and the private sector to address suburban development needs.

Beyond these larger accomplishments, the Partnership is continuing to evolve as an organization through its fifteenth year of operations and beyond. While the organization has benefitted from the continuity and leadership of a committed two member staff, resources have expanded through innovative new relationships. At the encouragement of several Northeastern University alumni, the Partnership is participating in the Northeastern Co-Op program, accessing diverse skills from college students. Enabled by state support secured by our legislators, the Partnership contracted with the University of Massachusetts at Dartmouth's Public Policy Center to provide extensive new data research and analysis on the region for the 495/MetroWest Suburban Edge Community Commission.

Paralleling these innovative changes, through visionary leadership our revenue streams have expanded to include municipal stipends, state funding secured by our legislators and supported by the Baker/Polito Administration, professional services, new grants, updated minimum contributions, and additional investors. Our collaborations have expanded as well, on a state level with the Executive Offices of Housing & Economic Development, Energy & Environmental Affairs, and Labor & Workforce Development, Massachusetts Department of Transportation, Massachusetts Department of Environmental Protection, Massachusetts Office of Business Development, and MassDevelopment; on a regional level with the Central Massachusetts Regional Planning Commission, CrossTown Connect Transportation Management Association, Metropolitan Area Planning Council, MetroWest Visitors Bureau, MetroWest Regional Transit Authority, Neponset Valley Transportation Management Association, Partnerships for a Skilled Workforce, and Worcester Regional Transit Authority; and locally, with municipal governments on development opportunities, grant applications, transportation projects, transit needs, stormwater, water infrastructure, housing, and many other fronts.

Through these strengthened relationships and resources, the Partnership has continued making a real difference in our thirty-five communities, working with both professional and elected municipal officials, legislators, business executives, state agency officials, developers, higher education institutions, environmentalists, and other regional leaders to address our regional constraints while highlighting our strengths.



PHOTO ABOVE: From left to right, State Representative Carolyn Dykema, State Representative Chris Walsh, State Representative Hannah Kane, State Representative David Muradian, then-MBTA Administrator Brian Shortsleeve, and Governor Baker at a commuter rail track replacement project in Ashland.

PHOTO LEFT: Speaker of the House Robert DeLeo addresses regional leaders at the Partnership's State House Day in the House Members Lounge.

PARTNERSHIP'S MISSION STATEMENT

The Partnership, through a unique public-private collaboration with businesses, municipalities, and other stakeholders, is the regional leader for creating an environment that prepares for and cultivates sustainable growth. We accomplish this by coordinating, educating, and advocating for solutions to regional constraints and limited natural resources.



TRANSPORTATION ACCOMPLISHMENTS

SPECIAL THANKS to our Transportation Committee members, led by Private Sector CoChair Robert Nagi from VHB and Public Sector CoChair Joseph Nolan from the MetroWest Regional Transit Authority

- Advancing the 495/90 interchange project to the design phase with a \$227 Million construction allocation in the state's capital plan
- Successfully advocating for all Electronic Tolling on the Massachusetts Turnpike, and working with the legislative delegation to secure a more equitable tolling structure and removal of tollbooths
- Supporting local and state approval for the pilot program to expand commuter rail services to Foxborough
- Preparing a new resource, *495/MetroWest on the Go*, to provide a regional transportation profile on commuting patterns and transportation options with corresponding municipal profiles
- Leading the development of a Transportation for Massachusetts (T4Mass) resource guide on innovative mobility, *Fast Forward: The Technology Revolution in Transportation and What It Means for Massachusetts*, and authoring a chapter on the suburban perspective
- Providing technical support to CrossTown Connect, the region's newest Transportation Management Association, and related shuttle efforts such as Maynard's bus to the Acton station
- Supporting the MetroWest Regional Transit Authority's Framingham Intermodal Center and Worcester Regional Transit Authority's Westborough shuttle
- Convening dialogues between the MetroWest RTA, Worcester RTA, and municipalities to identify shared opportunities
- Serving on the Executive Committee of T4Mass to provide perspective on transportation needs and public-private collaborations from the 495/MetroWest region
- Conducting outreach and providing technical assistance to the State Senate in their MassMoves initiative, a new component of the Massachusetts Senate's 2017 Commonwealth Conversations



PHOTO ABOVE: From left to right, State Representative Hannah Kane, State Representative Carolyn Dykema, Partnership Deputy Director Jessica Strunkin, and MBTA General Manager Luis Ramirez, riding the Worcester-Framingham Line to hear from the House delegation and address commuters' concerns. (Not pictured, but also participating were State Representatives Danielle Gregoire, Jack Lewis, and Alice Peisch.)

“Beals and Thomas, Inc. is proud to support the 495/MetroWest Partnership to advance the Partnership’s mission of advocating for solutions to matters of regional importance. The Partnership provides a unique opportunity to meet with business leaders, public leaders, regulatory offices, and environmental groups; to discuss issues in a respectful setting.”

—George Preble, President of Beals and Thomas, Inc.

ENERGY & SUSTAINABLE DEVELOPMENT

SPECIAL THANKS to our Energy and Sustainable Development Committee members, led by Public Sector CoChair Kristen Las from the Town of Shrewsbury and Private Sector CoChair Beth Weirling from Patriot Place

- Overseeing the fifth year of our regional employer survey with Framingham State University and our media partner, Gatehouse Media/MetroWest Daily News, to provide employer perspective to policymakers and for business development initiatives and holding a forum to share results and perspectives with business leaders, municipal officials, and policymakers
- Preparing and distributing our annual regional economic report, *Strength in Numbers*, to provide updates on regional payroll, employment, educational attainment, and other key data points with corresponding municipal profiles
- Developing a new resource, *Life Sciences In the 495/MetroWest Region: Existing Importance and Future Possibility*, to highlight our region's life science employers for business development initiatives
- Holding a 495/MetroWest Life Sciences Forum with Sunovion Pharmaceuticals, with Secretary of Housing and Economic Development Jay Ash, President and CEO Travis McCready from the Massachusetts Life Science Center, President and CEO Robert Coughlin from MassBIO, and Executive Director Meredith Harris from the Marlborough Economic Development Corporation
- Convening an energy forum for the region with Day Pitney LLP and hosted by EMC, with a program including Lieutenant Governor Karyn Polito, Secretary of Energy and Environmental Affairs Matthews Beaton, legislators, municipal officials, employers such as Staples, IBM, and EMC, and energy entities such as ISO New England, National Grid, Eversource, and Exelon
- Collaborating with Transwestern on our annual commercial real estate report on the region, for distribution to municipalities, legislators, and key constituencies
- Distributing *495/MetroWest Development Opportunities* to our municipal and private sector stakeholders to ensure regional participation in business site selections
- Providing regional economic development perspective as part of the state's Regional Workforce Skills Initiative, which identified priority occupations and industries on a regional and state level
- Participating in the state's Regional Economic Development Organization Program with the state's Office of Business Development to assist employers and communities
- Providing input and assistance for the Framingham Tech Park Committee
- Serving on state's Economic Assistance Coordinating Council to provide regional perspective on oversight of state's Economic Development Incentive Program
- Conducting outreach to solicit regional participation in *Choosing Massachusetts for Business: Key Factors in Location Decision-Making*, a report by the UMass Donahue Institute for MassEcon on factors in business location decisionmaking
- Advocating for regional inclusion in MassEcon's ReadyMass 100 list of top development locations



- Holding forums and providing technical assistance and support for municipal applications to state programs such as MassWorks, Community Compact, and Open for Business, as well as MassDevelopment's Commonwealth Places and Site Readiness initiatives

PHOTO LEFT: Secretary of Housing and Economic Development Jay Ash addressing regional leaders at the Partnership's Meet the Cabinet forum hosted by Sanofi Genzyme in Framingham.

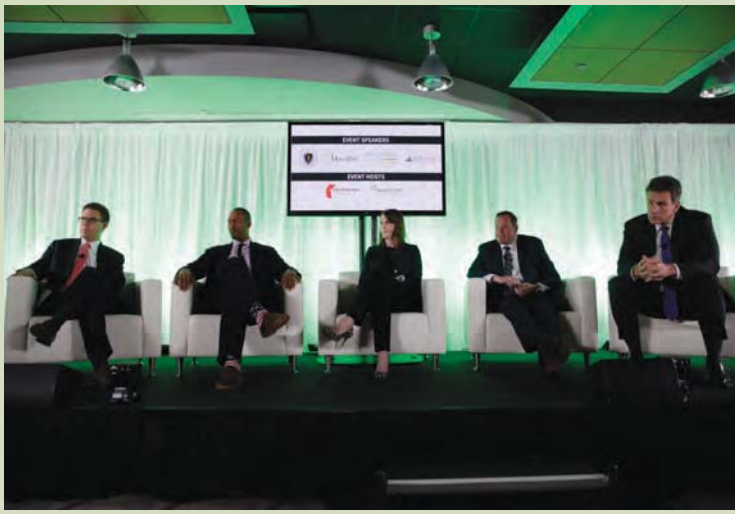


PHOTO ABOVE: The panel at the 495/MetroWest Life Sciences Forum; from left to right, Partnership Executive Director Paul Matthews; President & CEO of the Massachusetts Life Science Center Travis McCready; Marlborough Economic Development Corporation Executive Director Meredith Harris; Massachusetts Biotechnology Council President & CEO Robert Coughlin; and Secretary of Housing and Economic Development Jay Ash.

WATER RESOURCES

SPECIAL THANKS to our *Water Resources Committee members, led by Public Sector CoChair Brutus Cantoreggi from the Town of Franklin and Private Sector CoChair Gerry Preble from Beals and Thomas*

- Providing regional input and perspective to the U.S. Environmental Protection Agency on their proposed municipal separate storm sewer systems (MS4) permits for the region
- Hosting U.S. Environmental Protection Agency briefing when the final draft of the MS4 Permit was released
- Hosting a forum on the Massachusetts Clean Energy Center's Water Innovation work
- Hosting a discussion on Executive Office of Energy & Environmental Affairs' Water Policy including topics like the Interbasin Transfer Act and Drought Management
- Supporting the state's proposal to assume primacy of the National Pollutant Discharge Elimination System (NPDES) water quality permits, rather than continue to cede that role to the U.S. Environmental Protection Agency, given uncertainties on the national level; MA is currently one of only four states in the nation that does not have delegated authority to issue such permits
- Hosting an informational meeting with perspectives on NPDES primacy from the Massachusetts Department of Environmental Protection and Representative Carolyn Dykema
- Advocating for stronger state funding for municipal water supply, stormwater management, and wastewater treatment infrastructure via the recommendations of the state's *Water Infrastructure Finance Commission*
- Providing assistance and support to municipal governments developing innovative stormwater responses, including but not limited to the Minuteman Advisory Group on Interlocal Coordination (MAGIC – an MAPC Subregion) Stormwater Partnership and the Medway Integrated Water Resources Management Plan

“We, the Co-Chairs of this Commission, hope this report offers an understanding of today’s 495/MetroWest region and serves as a blueprint for the future... Our population and housing stock are growing faster than the rest of the state. Our residents are highly educated, with the skills needed for growing industries... Our economy is bolstered by innovative companies that represent the future of Massachusetts. And as a net labor importer, our region is home to more jobs than employed residents.”

495/METROWEST SUBURBAN EDGE COMMUNITY COMMISSION

SPECIAL THANKS to our legislative delegation and Secretary Jay Ash for their support of the Commission, as well as to the Co-Chairs— State Representative Kate Hogan, State Senator Karen Spilka, and Assistant Secretary of Communities and Programs Juan Vega from the Executive Office of Housing and Economic Development – as well as their associated staffs.



PHOTO ABOVE: From left to right, Secretary of Housing and Economic Development Jay Ash, Professor Michael Goodman, Executive Director of UMass Dartmouth's Public Policy Center, State Representative Kate Hogan, Senator Karen Spilka, Assistant Secretary for Communities and Programs Juan Vega, and Partnership Executive Director Paul Matthews, after discussing the Commission's work.

Our state legislative delegation created the 495/MetroWest Suburban Edge Community Commission with the support of the Baker/Polito Administration to provide a formal complement to the Partnership's work, convene a dialogue between state agencies and municipal officials, and call attention to the development needs of suburban communities.

First established via a House budget amendment by State Representative Kate Hogan and supported by the State Senate and

Baker/Polito Administration, this Commission is charged with analyzing challenges in suburban communities “such as needs to address transportation, water, cellular and energy infrastructure, transit services, residential development, reuse of former industrial facilities and historic mills, brownfields reclamation, downtown redevelopment and other constraints.”

The Commission was convened in 2016 by the three Co-Chairs, following the Partnership's selection of the University of Massachusetts at Dartmouth's Public Policy Center to conduct research and analysis of regional data to inform the Commission's deliberations and as a resource to the region.

In February 2018, the Commission finalized their initial report, entitled *495/MetroWest Suburban Edge Community Commission: Serving a “SuperNexus” for the Commonwealth's Economy*, which lays out blueprint for action by the state, local governments, and private sector through summaries of each policy area highlighting Commission discussions, emerging examples, and potential concepts to explore further. The Partnership is continuing to work with the Commission Co-Chairs to utilize the report by soliciting successful case studies, suggestions, and best practices on how to address the suburban development issues laid out as constraints for the 495/MetroWest region and other suburban communities.

KEY INDUSTRY CLUSTER CONCENTRATIONS

Location Quotients show industry concentrations in comparison to the U.S.

	495/MetroWest	Massachusetts
Biopharmaceuticals	11.16	3.18
Education and Knowledge Creation	2.27	5.97
Financial Services	1.07	2.81
Fishing and Fishing Products	0.18	5.81
Footwear	2.98	6.30
Information Technology and Analytics	15.14	5.59
Insurance Services	1.21	2.99
Jewelry and Precious Metals	0.31	3.99
Marketing, Design, and Publishing	2.18	2.59
Medical Devices	4.50	3.67
Recreational and Small Electric Goods	3.40	2.20

Infographic and data courtesy of the UMass Dartmouth Public Policy Center for the 495/MetroWest Suburban Edge Community Commission

GOING FORWARD

SINCE OUR FOUNDING, the Partnership has addressed regional challenges by bringing together our private and public sector leaders to forge consensus and collaborative solutions, which has paid enormous dividends on the region's behalf. The Partnership will continue playing a key role convening the region, creating a unified message with supporting data, and providing opportunities to work with state policymakers, through such ongoing initiatives as:

- ✓ **Supporting the 495/MetroWest Suburban Edge Community Commission's recommendations** – The Partnership is conducting outreach on the report and soliciting feedback to identify short and long term strategies to address suburban development challenges, with recommendations for municipalities, state, and the private sector.
- ✓ **Updating Priority Development Areas** – With support from MassDevelopment, the Partnership is working with the Central Massachusetts Regional Planning Commission and the Metropolitan Area Planning Council to reach out to municipal governments to update their locally selected priority sites. Last done in 2012, this initiative will also highlight any large industrial or commercial sites potentially suitable for MassDevelopment's Site Readiness Program.
- ✓ **Advancing the I-495/I-90 interchange redesign and construction** – While this project is in the design phase, the Partnership is supporting needed funding in the state's capital plan, while working with our legislators, MassDOT, municipal governments, employers, and other advocates to provide regional input into the project.
- ✓ **Supporting state primacy on NPDES permitting** – With ongoing uncertainty in Washington, our state, municipalities, and advocates need stability in a coordinated and Massachusetts based regulation of stormwater and wastewater issues.
- ✓ **Finding solutions to first and last mile connections** – With the pressure on the MBTA and state transportation network, regional innovations on providing first and last mile service must be supported, and the Partnership is highlighting and advocating for solutions.
- ✓ **Providing input in to the state's Commuter rail Vision Plan** – The Partnership is working with our legislators, municipalities, and employers to provide regional input for this strategic plan, while working with T4Mass and statewide organizations to advocate for common concerns.
- ✓ **Calling attention to other regional transportation infrastructure needs** – As identified in the Partnership's Top Ten Transportation Nightmares project, there are a number of other regional interchanges such as I-495/I-290 and I-495/ Rt. 9 and regional corridors such as Rt. 9 and Rt. 20 that need state investment and improvements.
- ✓ **Coordinating economic development and workforce development** – Through serving on our region's workforce investment board, Partnerships for a Skilled Workforce, and through working with Dean College, Framingham State University, MassBay, and the Tufts University Cummings School of Veterinary Medicine, the Partnership will provide economic development perspective for implementing the findings of the state's Regional Workforce Skills Initiative.
- ✓ **Building on regional messaging for life sciences** – The Partnership's new regional profile on life sciences provides a new means of improving coordination with municipalities and state agencies on life sciences initiatives.

Beyond these ongoing initiatives, most importantly the Partnership will continue our commitment to public-private collaboration, not only on a municipal and business level, but in our work with the 495/MetroWest legislative delegation and the Baker/Polito Administration. Our success has come about from our partnering within the region with key constituencies, as well as our partnering outside the region with policymakers, agencies, and advocacy organizations to advance the needs and interests of 495/MetroWest. While this success speaks for itself, significant challenges remain, and the Partnership's continued leadership is needed to ensure that the 495/MetroWest region continues to be positioned for success in the years ahead.

"The 495/MetroWest Partnership provides a crucial component for municipal governments to participate in a cohesive regional economic development effort. By providing vision, leadership and the opportunity to further public/private coordination, they continue to strengthen the 495/MetroWest corridor's economic presence while giving a voice for local governments to participate in regional economic efforts. The Partnership has been a tremendous advocate and support partner for several locally driven initiatives."

–Wrentham Town Administrator Kevin Sweet

SPECIAL THANKS to all of the regional leaders who have served on our Board of Directors, particularly our current officers:

Public Sector CoChair Jay Marsden, Holliston's Board of Selectmen
Private Sector CoChair Bill Pezzoni, Day Pitney LLP
Public Sector ViceChair Keith Bergman, Littleton's Town Administrator
Private Sector ViceChair Ellen Carlucci, UMass Memorial Marlborough Hospital
Treasurer Charles Dwyer, Middlesex Savings Bank
Clerk Peter Martin, Bowditch and Dewey LLP

PROVIDING A REGIONAL VOICE



PHOTO ABOVE: From left to right, Medway Selectman Glenn Trindade; Mark Buckley, Staples' Vice President of Environmental Affairs; Paul Fitzgerald, EMC's Vice President of Global Real Estate and Facilities; and Robert McDonald, IBM's Mass Lab Senior Location Executive speaking at the Partnership's Energy Forum

THE PARTNERSHIP provides the region's public and private sector leaders with opportunities to provide their perspective directly to government officials and agencies on a wide range of programs, such as:

➤ State House Day, for our regional leaders to meet with legislative leadership and our legislative delegation, with alternating addresses by the Senate President and Speaker of the House

➤ Meet the Cabinet with Secretary of Housing and Economic Development Jay Ash hosted by Sanofi Genzyme

➤ Meet the Cabinet with Secretary of Transportation Stephanie Pollack hosted by TJX

➤ Office of Governor Baker on the state's "Open for Business" initiative

➤ Executive Office of Housing & Economic Development & Division of Local Services on the state's "Community Compact" program

➤ Massachusetts Department of Transportation staff on commuter rail operations, transit, road and bridge investments, the Complete Streets program, and mobility

➤ Executive Office of Housing and Economic Development on the MassWorks program and Creative Economy initiative

➤ U.S. Environmental Protection Agency on stormwater permitting

➤ Massachusetts Executive Office of Energy and Environmental Affairs on the state's water policy programs

➤ Boston Metropolitan Area Planning Organization and Central Massachusetts Metropolitan Area Planning Organization on transportation improvement plans and long term regional transportation plans

➤ General Manager of Keolis on commuter rail operations

➤ 495/90 Working Group on developments affecting public and private stakeholders from the I-495/I-90 and I-495/Rt. 9 interchange

➤ MassDevelopment on their TA Connect, Property Assessed Clean Energy (PACE), and Site Readiness Programs

➤ State Representative Carolyn Dykema and Massachusetts Department of Environmental Protection on stormwater primacy

➤ Littleton Town Administrator Keith Bergman and VHB on opportunities for regional and municipal leadership on climate resiliency

In addition to these opportunities, the Partnership and our program committees – Transportation, Energy and Sustainable Development, and Water Resources – regularly convene regional forums on issues of interest.



495/METROWEST
PARTNERSHIP

Leaders for Regional Prosperity

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Deputy Director

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STRENGTH IN NUMBERS: 2018 EDITION

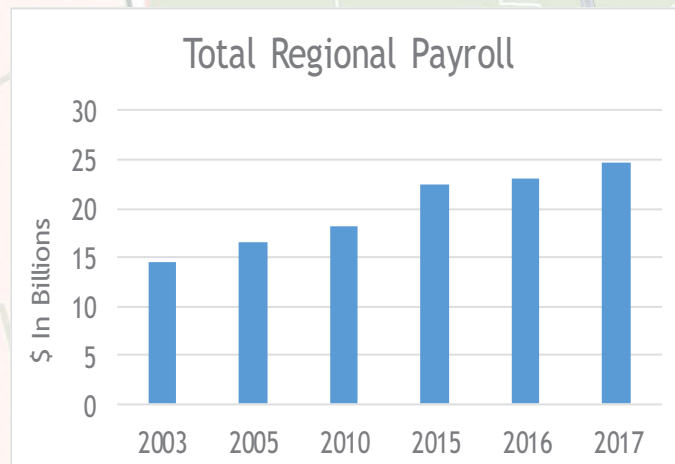
Demographic Data for the 495/MetroWest Region

The 495/MetroWest Partnership was founded in 2003 by business executives, municipal officials, legislators, and other leaders as a public-private economic development initiative, focused on securing needed policy decisions and infrastructure investments for our regional economy. The Partnership's service area has expanded by 3 communities since our founding 15 years ago, demonstrating the success of our regional identity and organizational accomplishments, and reflecting our communities' continued prosperity. The Partnership produces an annual update on the 495/MetroWest region's numbers, demonstrating the value of our 35 communities to the overall economic strength of the Commonwealth.

Whether considering our highly educated workforce, robust payroll numbers, jobs or employers, the 495/MetroWest region continues to offer a high return on investment for the Commonwealth, employers, and residents; a vibrant and diverse employment base; and an excellent quality of life. The 495/MetroWest Partnership is pleased to aggregate these important statistics in our effort to ensure the region's continued economic success and prosperity.

OVER \$24 BILLION ANNUAL PAYROLL

For the period between July 2016-June 2017, the total wages disbursed across all sectors of the economy in Massachusetts amounted to \$243.9 billion with approximately \$24.6 billion being generated within the 495/MetroWest region, equating to approximately **1 in every 10 payroll dollars**. Out of the state's \$12.8 billion increase in payroll over the previous twelve months, the 495/MetroWest region is responsible for \$1.5 billion. Moreover, the chart below reflects the total regional payroll since the year the Partnership was founded. For contextual purposes, in 1980, the regional economy had an annual payroll of \$2.6 billion.

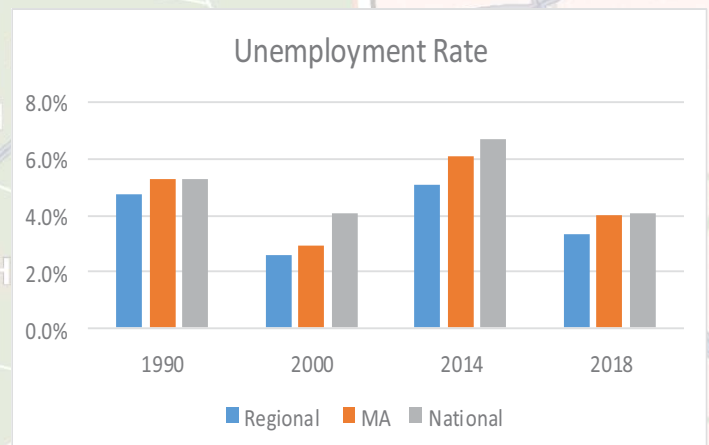


EMPLOYMENT: 1 IN 11 JOBS

The 35 communities in the 495/MetroWest region are home to approximately 1 in every 11 jobs in Massachusetts. According to the Massachusetts Department of Labor and Workforce for the period of July 2016-June 2017, a total of 3,519,173 individuals were employed across all sectors in the Commonwealth, with the **495/MetroWest region responsible for 334,165 jobs**. This is an increase from 330,822 jobs, which continues to trend upward, reflective of our region's and the state's consistent job growth over the last twelve months.

UNEMPLOYMENT: CONTINUES POSITIVE TREND

The 495/MetroWest region's unemployment rate has traditionally outperformed both the nation and the Commonwealth. As of February 2018, Massachusetts average unemployment was 4.0 and **495/MetroWest's average unemployment was 3.3%**, while the national unemployment rate was 4.1%. Compared to February 2017, the regional unemployment rate continued its positive trend, down from 3.4%.



REAL ESTATE ADVANTAGES: LOCATION, SUPPLY, PRICING

With a prime location accessible to Boston, Central Massachusetts, and Rhode Island, 495/MetroWest offers crucial advantages to employers. As noted in a recent market analysis by CBRE|New England for the Partnership, our region offers over 72 Million SF of office, lab, manufacturing, warehouse, and flex space, with office & life science properties growing by 16% in the last ten years. In addition to location and supply, our region has a significant pricing advantage, offering a 67% discount when compared to properties in the Boston and Cambridge markets.

STEADY POPULATION GROWTH

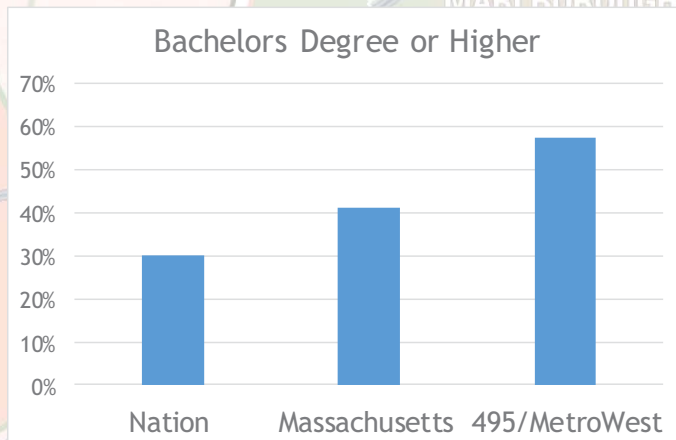
The 33 towns and the cities of Framingham and Marlborough comprising the 495/MetroWest region, stretch from Westford to Foxborough and Shrewsbury to Natick. Over a five-year period, the region has experienced a higher increase in total population at 4.22% when compared to the state average of 3.19%. The 2011 data showed a population of 592,057 in the 495/MetroWest Region. In 2016, the population of the Region is estimated to be 617,092.

SINGLE-FAMILY HOMES: COMPETITIVELY PRICED

According to the Warren Group, the median selling price for a single-family home in February 2018, within the 35 communities, was \$446,298. While higher than the median price for the state (\$340,000), the region continues to be more affordable than the Metro-Boston area.

WORKFORCE: HIGHLY EDUCATED

The 495/MetroWest region has a very well educated workforce with 57.6% of the population in possession of a bachelor's degree or higher. According to the U.S. Census Bureau, 41.2% of the Commonwealth's residents hold a bachelor's degree or higher for the same time frame and age group. Both Massachusetts and the 495/MetroWest region have higher levels of educational attainment than the nation-at-large at 30.3%.



EMPLOYERS: HOLDING STEADY

The 495/MetroWest region's employers range from small independent operations to family businesses, large employers and corporate headquarters. In the 2nd Quarter of 2016, there were a total of 21,013 reported establishments. By the 2nd Quarter of 2017, the number grew slightly to 21,354, which is a 1.6% change over the twelve-month period. While the pace of growth has slowed, the overall numbers demonstrate a sustainable trend.

BUSINESS CONFIDENCE: 71% FEEL POSITIVE

In October 2017, the Partnership, in collaboration with Framingham State University and media partner *MetroWest Daily News* conducted our fifth annual 495/MetroWest Business Climate Survey. This employer survey was developed to provide insights into our employers' perspectives on the regional economy, and was distributed throughout 495/MetroWest, with additional outreach from the Corridor Nine, Marlborough Regional, MetroWest, Milford Area, and United Chambers of Commerce. Based on the findings, more than two-thirds of responding employers believe the economy will improve in 2018; 48% plan to hire during the next year; and 15% plan to expand their facilities.

CONCLUSION:

While these numbers do not tell the whole story as to why the 495/MetroWest region is a premiere location for employers and residents, these statistics demonstrate our region's multiple strengths. The 495/MetroWest Partnership's work with municipal governments, employers, the Baker/Polito Administration, legislators, chambers of commerce, and other key stakeholders on behalf of the region utilizes our competitive advantages to leverage both public and private sector investment, ensuring continued economic prosperity for years to come.

If communities, employers, or others in the 495/MetroWest region are interested in further information or specific town data, please contact the Partnership staff at the number below or at info@495partnership.org.



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The 495/MetroWest Partnership is a public-private economic development organization that fosters our region's continued economic success and growth by advocating for needed investments and sustainable policy in transportation infrastructure, water resources and infrastructure, housing, and economic development.

For more information, visit www.495partnership.org.

Data is from the following sources: www.census.gov, Secretary of the Commonwealth, the U.S. Department of Commerce (Census), the MA Executive Office of Labor and Workforce Development, the MA Department of Revenue, CBRE/New England, the Warren Group, *MetroWest Daily News*, Dr. Michael Harrison of Framingham State University, Mass.gov Labor and Workforce Development, U.S. Census Bureau "2012-2016 American Community Survey 5-Year Estimates", and UMASS Donahue Institute "2016 Census Bureau Sub-County Population Estimates for Massachusetts".

Researched and prepared by Carter Sigl and Oscar Eale, both of the Northeastern University Co-op Program, in coordination with Partnership staff.