

SUBDIVISION AREA CHART

TOTAL AREA OF EXIST. LAND = 553,447 SQ. FT.
TOTAL NUMBER OF LOTS & PARCELS = 4
TOTAL AREA OF LOTS & PARCELS = 553,447 SQ. FT.
TOTAL AREA DEDICATED TO STREET PURPOSES = 25,592 SQ. FT.
TOTAL AREA DEDICATED TO EASEMENTS = 21,569 SQ. FT.
TOTAL AREA DEDICATED TO OPEN SPACE = 0
TOTAL SUM OF ABOVE 1,148,820 = TO 553,447 SQ. FT.

HILL VIEW ESTATES PERMANENT PRIVATE ROAD

MEDWAY ZONE AR-I

MIN. FRONTAGE 180 FT.
FRONT SETBACK 35 FT.
SIDE SETBACK 15 FT.
REAR SETBACK 15 FT.
MIN. LOT AREA 44,000 SQ. FT.

GENERAL NOTES

- ALL IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH MASS DOT HANDICAP REQUIREMENTS AND THE CURRENT ADA/AAB REQUIREMENTS IN EFFECT AT THE TIME OF CONSTRUCTION.
- THE PROPOSED LOCATIONS FOR THE ELECTRIC, TELEPHONE AND CABLE CONDUIT'S SHALL BE DETERMINED BY THE UTILITIES SERVICE PROVIDER.

APPROVED WAIVER LIST

7.6.2 B) WATER FACILITIES INSTALLATION
REQUEST WAIVER FROM INSTALLATION OF WATER MAIN, HYDRANT AND VALVES AND OTHER FITTINGS REQUIRE FOR WATER MAIN TIE IN.

7.7.2 P) STORM WATER
REQUEST WAIVER FROM REQUIRED 30 FT SETBACK FROM PROPERTY LINES

7.9.5 B) GRADE
REQUEST WAIVER FROM REQUIRED PERMANENT PRIVATE WAY MAX. CENTERLINE ROAD GRADE OF 8% TO EXISTING GRADE OF 10.8%.

7.9.7 G) ROADWAY LOCATION
REQUEST WAIVER FROM REQUIRED PERMANENT PRIVATE WAY MIN. 18 FT. PAVEMENT WIDTH. TO EXISTING 12 FT OF PAVEMENT AND 3 FT OF CRUSHED STONE ON SHOULDERS.

7.9.7 (H) ROADWAY CONSTRUCTION
REQUEST WAIVER FROM REQUIRED INSTALLATION OF HOT MIX ASPHALT ROAD PAVEMENT@ HAMMER HEAD AREA.

7.10.1 VERTICAL GRANITE CURBING
REQUEST WAIVER FROM REQUIRED INSTALLATION OF VERTICAL GRANITE CURBING AT CUL-DE-SAC ENTRANCE

7.10.2 CURBS AND BERMS
REQUEST WAIVER FROM REQUIRED INSTALLATION OF HOT MIX ASPHALT CAPE COD BERM.

7.17.1 FIRE PREVENTION
REQUEST WAIVER FROM REQUIRED INSTALLATION OF FIRE ALARM SYSTEM.

7.19.2 TREES AND SLOPE STABILIZATION
REQUEST WAIVER FROM REQUIRED INSTALLATION OF TREES ALONG PROPOSED PRIVATE ROAD.

7.21 STREET LIGHTS
REQUEST WAIVER FROM REQUIRED INSTALLATION OF STREET LIGHTS.

NOTE: PRESENT AND FUTURE OWNERS ARE SUBJECT TO A PRIVATE ROAD MAINTENANCE AGREEMENT, RECORDED AT THE NORFOLK COUNTY REGISTRY OF DEEDS IN BOOK 36564, PAGE 548.

NOTE:

THE SUBJECT PROPERTY IS NOT CLASSIFIED AS CHAPTER 61A OR 61B.
THE SUBJECT PROPERTY DOES NOT LIE WITHIN THE FLOOD PLAIN.
HILL STREET IS AN SCENIC ROAD

STREET INFORMATION

| HILL STREET | PUBLIC VARIABLE WIDTH | 21 FT WIDE PAVEMENT (SCENIC ROAD) |
|------------------|-----------------------|-----------------------------------|
| SKYLINE DRIVE | 45 FT PUBLIC WAY | 19 FT. WIDE PAVEMENT |
| FAIRWAY LANE | 50 FT PUBLIC WAY | 26 FT WIDE PAVEMENT |
| NESTLEBROOK LANE | 50 FT PRIVATE WAY | 19 FT WIDE PAVEMENT |
| VICTORY LANE | 50 FT PRIVATE WAY | 10 FT WIDE PAVEMENT |

NOTE:

This plan is subject to the Certificate of Action recorded at the Norfolk County Registry of Deeds at Book 35648 Page 237, and to the Certificate of Action which shall be recorded with this Modified Definitive Subdivision Plan at the Norfolk County Registry of Deeds.

The present and future owners of Lots 10C & 10B are to provide a performance surety for the construction of ways and installation of municipal services in accordance with General Laws chapter 41 Section 81U.

The house address numbers shall be marked on a post at the end of each house lot driveway to facilitate easy identification from the private roadway for emergency and safety vehicles.

LEGAL REFERENCE

PL. BK. 275 PL # 408
PL. BK. 286 PL # 1027
PL. BK. 480 PL # 668-2000
DEED BK 38021 PG 60
DEED BK 14558 PG 459
PL. BK. 664 PL # 6-14
PL. BK. 679 PL # 15-16
APPROVAL BK 35648 PG 237
DEED BK 41094 PG 262
DEED BK 41552 PG 210

REVISION DATE: OCTOBER 28, 2013
COMMENTS FROM FEBRUARY 7, 2013
FROM PGC ASSOCIATES & TETRA TECH

REVISION DATE: MARCH 25, 2014
COMMENTS FROM CERTIFICATE OF ACTION

REVISION DATE: OCTOBER 10, 2017

REVISION DATE: JANUARY 9, 2024
NEW STORMWATER REGULATIONS UPDATE

REVISION DATE: FEBRUARY 22, 2024
REVISIONS PER TOWN COMMENTS

REVISION DATE: MARCH 8, 2024
REVISIONS PER TOWN COMMENTS

I CERTIFY THAT THIS PLAN HAS BEEN PREPARED IN CONFORMITY WITH THE RULES AND REGULATIONS OF THE REGISTERS OF DEEDS OF THE COMMONWEALTH OF MASSACHUSETTS

DATE: _____

APPROVAL UNDER SUBDIVISION CONTROL LAW IS REQUIRED.

DATE APPROVED: _____

MEDWAY PLANNING BOARD

DATE ENDORSED: _____

I CERTIFY THAT 20 DAYS HAVE ELAPSED SINCE PLANNING BOARD APPROVAL, AND NO APPEAL HAS BEEN FILED IN THIS OFFICE.

DATE: _____

MEDWAY TOWN CLERK

HILL VIEW ESTATES

DEFINITIVE SUBDIVISION
PERMANENT PRIVATE WAY

PLAN OF LAND
IN
MEDWAY, MA
MAP 3 PARCELS 33 & 33-01
ZONING AR1 DISTRICT

OWNER 1/APPLICANT:
ONE NIRVANA DEVELOPMENT LLC
741 WASHINGTON STREET
CANTON, MA 02021

OWNER 2:
TWO NIRVANA WAY REAL ESTATE TRUST
2 NIRVANA WAY
MEDWAY, MA 02053

DATE: MARCH 12, 2012

COLONIAL ENGINEERING, INC.
11 AWL STREET
MEDWAY, MA 02053
508-533-1644

LEGACY ENGINEERING LLC
730 MAIN STREET (SUITE #2C)
MILLIS, MA 02054
508-376-8883

0' 200' 400' 600'
SCALE 1" = 200'

COVER SHEET

SHEET 1 OF 10



Digitally signed by Daniel J. Merrikin, P.E.
Date: 2024.03.08 12:42:20 -0500

NOTE: LOT 10B AND PARCEL A BEING A SUBDIVISION OF LOT 10 AS SHOWN ON A PLAN BY CARLSON SURVEY COMPANY, DATED APRIL 1, 2000 RECORD AT NORFOLK COUNTY REGISTRY OF DEEDS IN PL. BK #480 PLAN # 668. LOT 10C & PARCEL C BEING A SUBDIVISION OF LOT 10A AS SHOWN ON A PLAN RECORDED AT THE NORFOLK COUNTY REGISTRY OF DEEDS IN PL. BK. #664 PLAN #6-14

These standard symbols will be found in the drawing.

 REBAR TO BE SET
 CONCRETE MONUMENTS TO BE SET

| | |
|---------------|----------------|
| MIN. FRONTAGE | 180 FT |
| FRONT SETBACK | 35 FT |
| SIDE SETBACK | 15 FT |
| REAR SETBACK | 15 FT |
| MIN. LOT AREA | 44,000 SQ. FT. |

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DATE: _____

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DATE ENDORSED: _____

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SCALE 1" = 40'

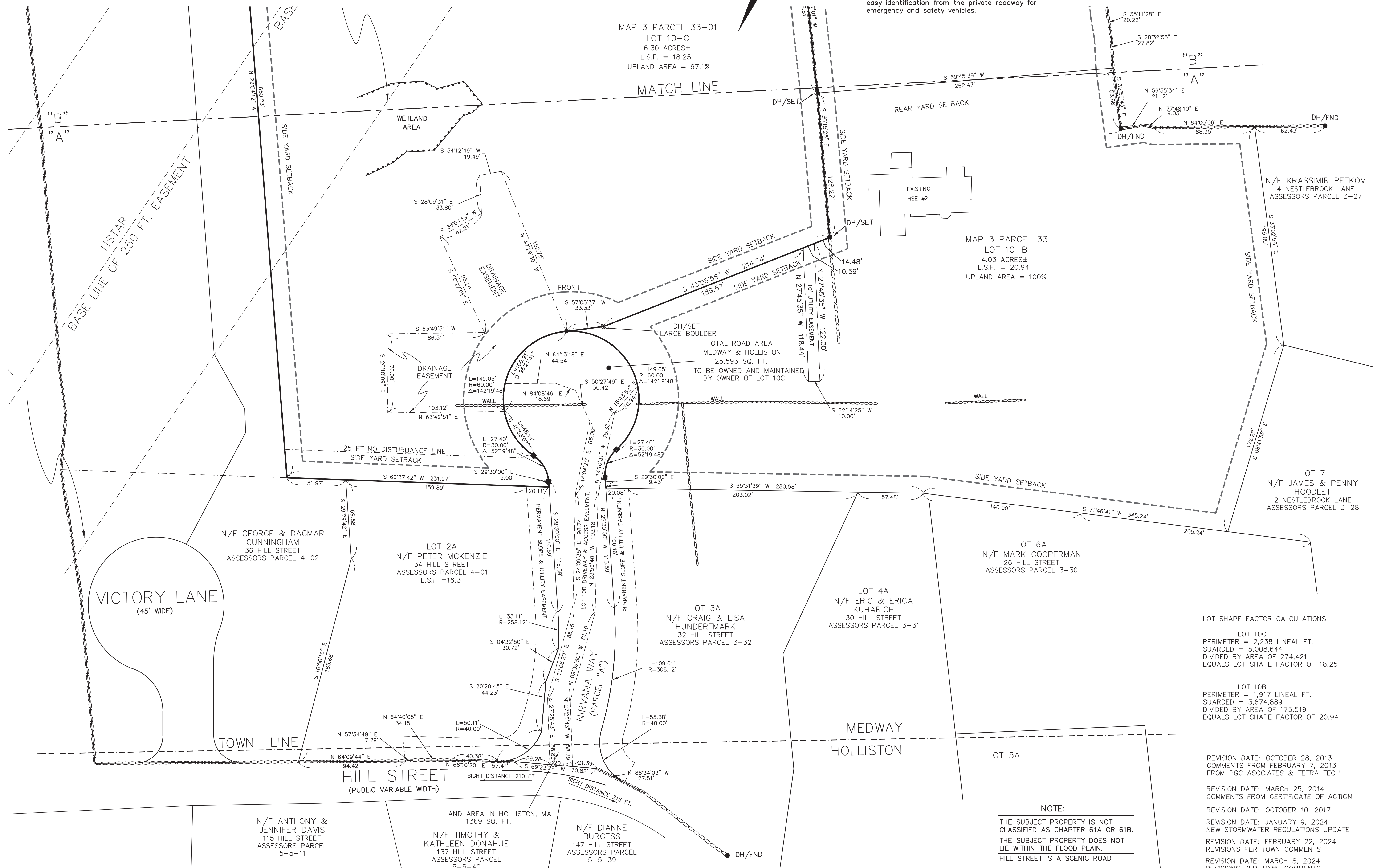
LEGAL SHEET

SHEET 2 OF 10

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LEGEND

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- ◆ REBAR TO BE SET
■ CONCRETE MONUMENTS TO BE SET

MEDWAY ZONE AR-1

MIN. FRONTAGE 180 FT.
FRONT SETBACK 35 FT.
SIDE SETBACK 15 FT.
REAR SETBACK 15 FT
MIN. LOT AREA 44,000 SQ. FT.

LOT SHAPE FACTOR CALCULATIONS

LOT 10C
PERIMETER = 2,238 LINEAL FT.
SUARDED = 5,008,644
DIVIDED BY AREA OF 274,421
EQUALS LOT SHAPE FACTOR OF 18.25

LOT 10B
PERIMETER = 1,917 LINEAL FT.
SUARDED = 3,674,889
DIVIDED BY AREA OF 175,519
EQUALS LOT SHAPE FACTOR OF 20.94

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N/F JOHN & SUSAN
BOCZANOWSKI
18 SKYLINE DRIVE
ASSESSORS PARCEL 8-31

N/F FRANK & DEBI ROSSI
20 SKYLINE DRIVE
ASSESSORS PARCEL 8-30

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MEDWAY TOWN CLERK

HILL VIEW ESTATES

DEFINITIVE SUBDIVISION
PERMANENT PRIVATE WAY

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IN
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FROM PGC ASSOCIATES & TETRA TECH

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REVISION DATE: FEBRUARY 22, 2024
REVISIONS PER TOWN COMMENTS

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HILL STREET IS A SCENIC ROAD

0' 40' 80' 120'
SCALE 1" = 40'
LEGAL SHEET
SHEET 3 OF 10

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LEGEND

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- D EXISTING DECIDUOUS TREE
O EXISTING OAK TREE
M EXISTING MAPLE TREE
C EXISTING CEDAR TREE
DEAD? EXISTING LIKELY DYING DECIDUOUS TREE

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Digitally signed by Daniel J. Merrikin, P.E.
Date: 2024.03.08 12:42:40 -05'00'

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MEDWAY TOWN CLERK

HILL VIEW ESTATES

DEFINITIVE SUBDIVISION
PERMANENT PRIVATE WAY

PLAN OF LAND
IN
MEDWAY, MA

MAP 3 PARCELS 33 & 33-01

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508-533-1644

LEGACY ENGINEERING LLC
730 MAIN STREET (SUITE #2C)
MILLIS, MA 02054
508-376-8883

EXISTING CONDITIONS NOTES:

1. EXISTING UTILITY INFORMATION IS BASED ON AVAILABLE INFORMATION AND IS NOT WARRANTY TO BE ACCURATE WHERE FEATURES ARE BURIED. CONTRACTOR CONTACT DISAFA PRIOR TO ANY EXCAVATION ACTIVITY AND SHALL CONFIRM ALL UTILITY LOCATIONS WITH APPLICABLE UTILITY COMPANIES.

2. EXISTING CONDITIONS INFORMATION FOR OFF-SITE PROPERTIES ARE NOT BASED ON FIELD SURVEY AND ARE TAKEN FROM MASSGIS AERIAL PHOTOGRAPHY.

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SCALE 1" = 40'

EXISTING CONDITIONS

SHEET 4 OF 10

EXISTING CONDITIONS NOTES:

1. EXISTING UTILITY INFORMATION IS BASED ON AVAILABLE INFORMATION AND IS NOT WARRANTY TO BE ACCURATE WHERE FEATURES ARE BURIED. CONTRACTOR CONTACT DIGSAFE PRIOR TO ANY EXCAVATION ACTIVITY AND SHALL CONFIRM ALL UTILITY LOCATIONS WITH APPLICABLE UTILITY COMPANIES.
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Date: 2024.03.08 12:42:59 -05'00'

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MEDWAY TOWN CLERK

HILL VIEW ESTATES

DEFINITIVE SUBDIVISION
PERMANENT PRIVATE WAY

PLAN OF LAND
IN
MEDWAY, MA

MAP 3 PARCELS 33 & 33-01

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NEW STORMWATER REGULATIONS UPDATE

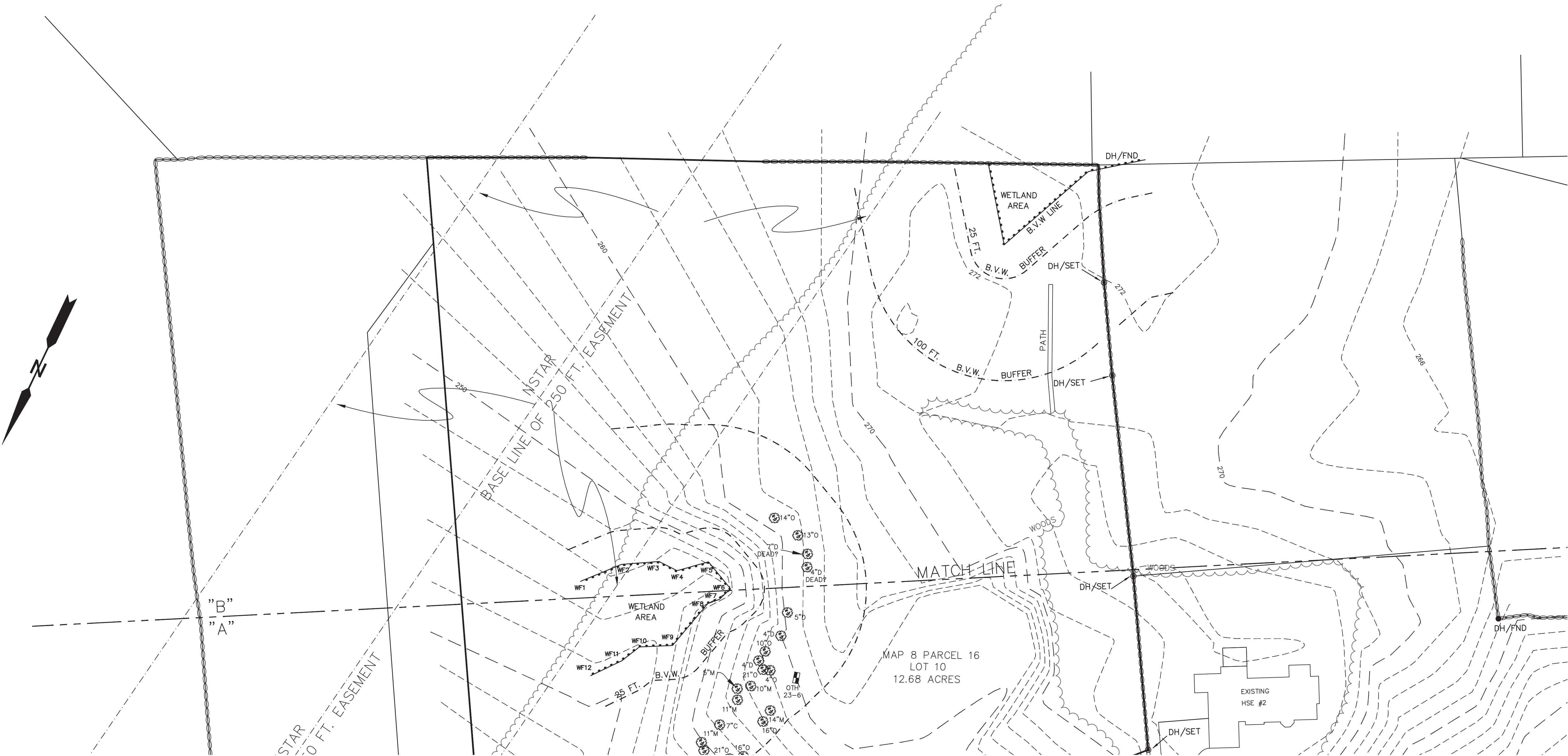
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0' 40' 80' 120'

SCALE 1" = 40'

EXISTING CONDITIONS

SHEET 5 OF 10

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NOTE: SITE IS NOT ACCESSABLE TO TOWN WATER OR SEWER PRIVATE WELL AND SEPTIC TO BE INSTALLED ON LOT 10C

LEGEND

These standard symbols will be found in the drawing.

- SM SUGAR MAPLE TREE TO BE PLANTED
- WO WHITE OAK TREE TO BE PLANTED
- AB AMERICAN BEECH TREE TO BE PLANTED

NOTE:

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTORS TO VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION DIG SAFE IS TO BE NOTIFIED 72 WORKING HOURS IN ADVANCE OF CONSTRUCTION DIG SAFE 1-888-DIG-SAFE



Digitally signed by Daniel J. Merrikin, P.E.
Date: 2024.03.08 12:46:12 -05'00'

I CERTIFY THAT THIS PLAN HAS BEEN PREPARED IN CONFORMITY WITH THE RULES AND REGULATIONS OF THE REGISTERS OF DEEDS OF THE COMMONWEALTH OF MASSACHUSETTS

DATE: _____

CUT/FILL:

- TOTAL CUT: 1,600 C.Y.
- TOTAL FILL: 1,100 C.Y.
- NET: 500 C.Y. CUT

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0' 40' 80' 120'

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PROPOSED GRADING

SHEET 6 OF 10

PLANTING NOTES:

1. SHRUBS SHALL BE INSTALLED IN AT LEAST THE SAME QUANTITIES AS SHOWN. SHRUB SPECIES SUBSTITUTION IS ALLOWED IF APPROVED BY CONSERVATION AGENT.

STORMWATER PLANTING LEGEND

- ARONIA MELANOCARPA (BLACK CHOKEBERRY)
- RHODODENDRON CANADENSE (RHODORA)
- VIBURNUM ACERIFOLIUM (MAPLELEAF VIBURNUM)
- SPIRAEA LATIFOLIA (MEADOWSWEET)
- MIXTURE OF I. VERTICILLATA (WINTERBERRY), CEPHALANTHUS OCCIDENTALIS (BUTTONBUSH), CHAMAEDAPHNE CALYCVLATA (LEATHER-LEAF) ALNUS SERRULATA (SMOOTH ALDER)

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TREES IN THE WETLAND BUFFER ZONE:

AS SHOWN ON SHEET 4, THE TREES PROPOSED TO BE REMOVED ARE AS FOLLOWS:

- 3"-8" DIA
1.1. 3 DECIDUOUS
1.2. 1 OAK
1.3. 3 MAPLE
1.4. 1 CEDAR
- 8"-20" DIA.
2.1. 1 DECIDUOUS
2.2. 6 OAK
2.3. 4 MAPLE
- >20" DIA.
3.1. 2 OAK

THE PROPOSED TREE REPLACEMENTS ARE:

- 6 DECIDUOUS (AMERICAN BEECH)
- 11 OAK (WHITE, BLACK, OR RED)
- 7 MAPLE (RED OR SUGAR)
- 3 CEDAR (EASTERN RED OR ATLANTIC WHITE)
- 18 SHRUBS PER SCHEDULE

- REPLACEMENT TREES ARE TO BE AT LEAST 1" CALIPER, WELL GROWN, WELL SHAPED AND BUSHY, WITH WELL SPACED BRANCHES GIVING IT WEIGHT AND GOOD BUD QUALITY.
- TREES SHALL BE PLACED AS SHOWN ON THE PLANS WITH THE NUMBER OF CHOSEN SPECIES MATCHING THE ABOVE LIST. WHICH SPECIES IS PLANTED IN WHICH LOCATION SHALL BE UP TO THE DEVELOPER.
- FOR TREES PLANTED IN THE R.O.W., PLEASE REFER TO THE LEGEND AT THE TOP OF THIS SHEET.

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- NOTES:
1. THE TOTAL AREA TO BE DISTURBED IS APPROXIMATELY 1.5 ACRES.
 2. EXISTING SOILS ARE CANTON CLASS B SOILS AND RIDGEBURY CLASS C SOILS.
 3. ALL PROPOSED SLOPES ARE TO BE MAXIMUM 3:1.

SEDIMENT & EROSION CONTROL NOTES

GENERAL

1. FOR COMPLETE EROSION CONTROL AND CONSTRUCTION ACTIVITY GUIDANCE, REFER TO THE STORMWATER POLLUTION PREVENTION PLAN ("SWPPP").
2. FOR SWPPP PROJECTS, A NOTICE SHALL BE POSTED VISIBLE FROM A PUBLIC RIGHT-OF-WAY WITH CONTACT INFORMATION AND INSTRUCTIONS TO FIND A COPY OF THE SWPPP.

PHASING AND CONSTRUCTION SEQUENCE

1. PHASE 1 - CONSTRUCTION SEQUENCE
 - 1.1. STAKEOUT AND INSTALL EROSION AND PERIMETER CONTROLS (WHICH WILL DELINEATE THE LIMIT OF WORK FOR THE PROJECT);
 - 1.2. DEMARCATe THE LOCATION OF EXISTING UTILITIES ON AND AROUND THE CONSTRUCTION AREA;
 - 1.3. CLEAR THE PROPOSED ENTRY AREAS AND INSTALL THE CONSTRUCTION ENTRANCE APRON(S);
 - 1.4. CLEAR AND GRUB THE DEVELOPMENT AREA;
 - 1.5. STRIP AND STOCKPILE TOPSOIL;
 - 1.6. COMMENCE ROUGH GRADING TO SUB-GRADE ELEVATIONS;
 - 1.7. INSTALL BUILDING FOUNDATION(S) AND BEGIN BUILDING CONSTRUCTION;
 - 1.8. INSTALL NEW SITE UTILITIES, INCLUDING THE STORMWATER MANAGEMENT SYSTEM;
 - 1.9. COMPLETE CONSTRUCTION OF NEW BUILDING(S);
 - 1.10. PAVE THE ROAD/DRIVEWAY AND SIDEWALK (IF APPLICABLE) TO BINDER COURSE;
 - 1.11. LOAM, SEED, AND STABILIZE DISTURBED AREAS, INCLUDING ALL SITE LANDSCAPING;
 - 1.12. CLEANOUT ALL CATCH BASINS AND NEW STORMWATER BMPs IN THE PORTION OF THE SITE AFFECTED BY CONSTRUCTION ACTIVITIES AFTER THE SITE IS FULLY STABILIZED.

PERIMETER EROSION CONTROL AND SEDIMENT CONTROLS

1. PERIMETER EROSION AND SEDIMENT CONTROL BARRIERS WILL BE PROVIDED, INSTALLED, AND MAINTAINED DOWNSIDE OF ALL PROPOSED CONSTRUCTION ACTIVITIES IN ACCORDANCE WITH THE SWPPP (IF APPLICABLE), THE SITE PLAN, AND ALL PERMITS ISSUED FOR THE SITE DEVELOPMENT. SUCH CONTROLS MUST BE INSTALLED BEFORE ANY EARTH-DISTURBING ACTIVITIES OCCUR ON THE SITE IN QUESTION. EROSION AND SEDIMENT CONTROLS MAY BE INSTALLED IN PHASES SO LONG AS IT PRECEDES ANY EARTH-DISTURBING ACTIVITIES WITHIN THE CONTROLS' UPSTREAM WATERSHED.
2. SEDIMENT SHALL BE REMOVED ALONG SUCH CONTROLS ON A REGULAR BASIS. IN NO CASE, SHALL SEDIMENT BE ALLOWED TO REACH A DEPTH EQUAL TO ONE HALF OF THE ABOVE GROUND HEIGHT OF THE EROSION CONTROL DEVICE.

SITE ACCESS CONTROLS

1. CONSTRUCTION VEHICLES WILL USE DESIGNATED ENTRY POINTS FOR EACH SITE. CRUSHED STONE OR RIP-RAP ENTRY APRON(S) WILL BE INSTALLED AND PROPERLY MAINTAINED DURING CONSTRUCTION UNTIL THE SITE IS PAVED. ALL CONSTRUCTION ACCESS WILL BE VIA THE FRONTAGE PUBLIC ROAD STREET, WHICH WILL BE KEPT CLEAN AND SWEEPED AS NEEDED TO MINIMIZE THE TRACKING OF SOILS AND DUST FROM THE SITE.

STOCKPILED SOILS

1. SOIL STOCKPILES TO BE LEFT IN PLACE MORE THAN 24 HOURS SHALL BE SURROUNDED WITH A LINE OF SILT FENCE OR COMPOST SOCK TO PREVENT THE PILES FROM ERODING INTO THE SITE AND TO DISCOURAGE ON-SITE RUNOFF FROM ERODING THE STOCKPILES. SOIL STOCKPILES TO BE LEFT IN PLACE MORE THAN 14 DAYS SHALL BE STABILIZED TEMPORARILY. DUST CONTROL MEASURES SHALL BE IMPLEMENTED TO PREVENT WIND EROSION OF THE STOCKPILES.

DUST CONTROL

1. DUST CONTROL MEASURES WILL BE IMPLEMENTED REGULARLY TO PREVENT THE OFF-SITE DEPOSITION OF WIND-ERODED SOILS. THE PRINCIPAL FORM OF DUST CONTROL WILL BE WATER APPLICATION.

DISTURBANCE OF STEEP SLOPES

1. CONTRACTORS MUST PAY CAREFUL ATTENTION TO STEEP SLOPES AND MUST IMPLEMENT ADDITIONAL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES DURING WORK ON STEEP SLOPES TO PREVENT EROSION. INSTALL EROSION CONTROL BLANKETS IF NEEDED.

SOIL COMPACTION

1. AREAS DESIGNATED FOR FINAL VEGETATIVE SURFACES OR CONSTRUCTION-STAGE OR FINAL STORMWATER INFILTRATION PRACTICES SHALL BE PROTECTED FROM EXCESSIVE COMPACTION BY RESTRICTING VEHICLE ACCESS AND THE TYPES OF EQUIPMENT THAT MAY BE USED IN SUCH AREAS. PRIOR TO SEEDING/PLANTING OF SUCH AREAS, EXPOSED SOIL THAT HAS BEEN COMPACTED SHALL BE LOOSENEED BY TILLING OR OTHER SIMILAR METHODS. CONDITIONING SHALL CONSIST OF DEEP TILLING WITH A ROTARY TILLER, DISC HARROWING, OR MANUAL LOOSENING AND RE-GRADING WITH AN EXCAVATOR BUCKET. CONDITIONING SHALL EXTEND TO A DEPTH OF AT LEAST 12-INCHES.

PROTECTION OF STORM DRAIN INLETS

1. ALL STORM DRAIN SYSTEM INLETS INSIDE OF PERIMETER CONTROLS SHALL BE PROTECTED WITH SEDIMENT CONTROL MEASURES DESIGNED TO REMOVE SEDIMENT FROM STORMWATER PRIOR TO ENTERING THE INLET. CATCH BASINS ALONG THE STREET FRONTAGE SHALL ALSO BE PROTECTED. SUCH MEASURES SHALL BE PERIODICALLY MAINTAINED AND REPLACED AS NEEDED TO ENSURE THEIR PROPER FUNCTIONALITY. SEDIMENT SHALL BE REMOVED DAILY WHERE FOUND.

TEMPORARY STABILIZATION

1. FOR PERMANENTLY OR TEMPORARILY (14 DAYS) CEASED CONSTRUCTION ACTIVITIES, TEMPORARY SOIL STABILIZATION MEASURES WILL BE EMPLOYED AS SOON AS PRACTICABLE BUT NO LONGER THAN 14 CALENDAR DAYS AFTER STABILIZATION HAS BEEN INITIATED. WHERE APPLICABLE, SEE THE SWPPP FOR ADDITIONAL REQUIREMENTS.

MAINTENANCE OF EROSION & SEDIMENT CONTROL MEASURES

1. EROSION AND SEDIMENT CONTROL MEASURES WILL BE MAINTAINED IN GOOD CONDITION FOR THE DURATION OF THE CONSTRUCTION ACTIVITY AND UNTIL SUCH TIME AS THE UPSTREAM AREAS ACHIEVE FINAL STABILIZATION. SEDIMENT WILL BE REMOVED ALONG PERIMETER EROSION CONTROLS WHEN THE DEPTH EXCEEDS FOUR-INCHES. ALL CONTROL MEASURES WILL BE MAINTAINED IN EFFECTIVE OPERATING CONDITION. IF SITE INSPECTIONS IDENTIFY CONTROL MEASURES THAT ARE NOT OPERATING EFFECTIVELY OR FINDS OTHER PROBLEMS, THE CONTRACTOR MUST:
 - 1.1. INITIATE WORK TO CORRECT THE PROBLEM IMMEDIATELY UPON DISCOVERY AND COMPLETE THE WORK BY THE CLOSE OF THE NEXT WORK DAY IF THE PROBLEM CAN BE CORRECTED THROUGH ROUTINE MAINTENANCE.
 - 1.2. FOR MORE SIGNIFICANT REPAIRS OR WHERE INSPECTIONS DETERMINE THAT ADDITIONAL EROSION AND SEDIMENT CONTROLS ARE NEEDED, SUCH WORK MUST BE COMPLETED AND OPERATION NO LATER THAN 7 CALENDAR DAYS AFTER DISCOVERY OF THE PROBLEM.

POLLUTION PREVENTION

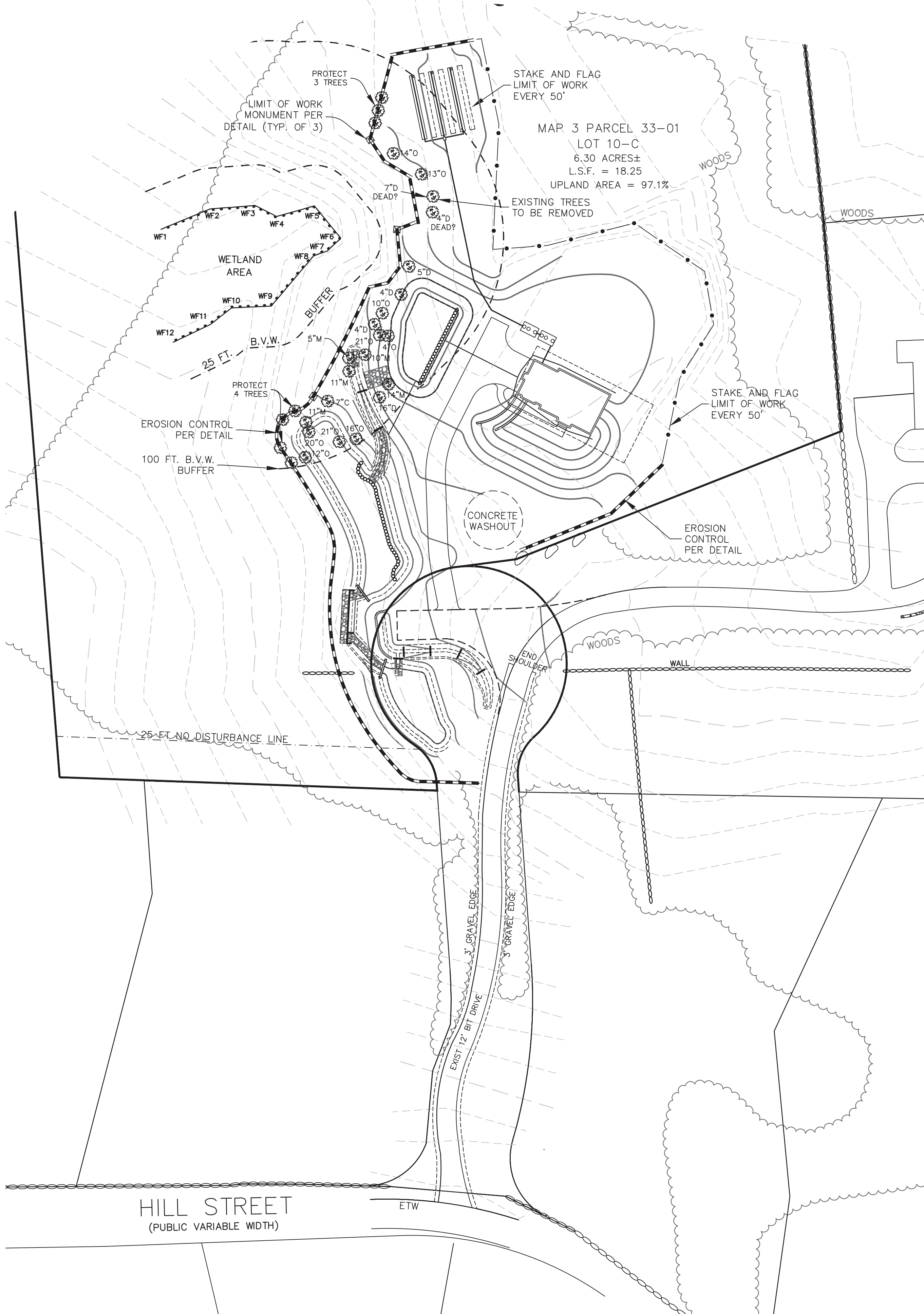
1. CONSTRUCTION STAGING AREAS WILL BE LIMITED IN QUANTITY AND WILL BE MAINTAINED IN A NEAT AND ORDERLY FASHION. REFER TO THE SITE PLAN FOR STAGING AREA LOCATION(S).
2. VEHICLES SHALL BE STORED IN FUELED IN DESIGNATED AREAS. MONITOR ALL EQUIPMENT FOR LEAKS. WHERE APPLICABLE, REFER TO THE SWPPP FOR ADDITIONAL VEHICLE STORAGE AREA REQUIREMENTS.
3. VEHICLE OR EQUIPMENT WASHING IS NOT ALLOWED ON-SITE.
4. MATERIALS AND EQUIPMENT SHALL BE STORED PROPERLY AND ORDERLY.
5. WHERE APPLICABLE, REFER TO THE SWPPP FOR:
 - 5.1. FURTHER INSTRUCTIONS ON PROPER MATERIAL AND EQUIPMENT STORAGE.
 - 5.2. REFER TO THE SWPPP FOR DISPOSAL OF WASTE, INCLUDING HAZARDOUS WASTE.
 - 5.3. REFER TO THE SWPPP FOR SPILL PREVENTION AND RESPONSE PROCEDURES.

DEWATERING PRACTICES

1. THIS SITE IS NOT EXPECTED TO ENCOUNTER SIGNIFICANT QUANTITIES OF GROUNDWATER DURING CONSTRUCTION ACTIVITIES BUT IF IT DOES, THE FOLLOWING PRACTICES WILL BE IMPLEMENTED:
 - 1.1. DO NOT DISCHARGE ANY FLOATING SOLIDS OR FOAM;
 - 1.2. IF DEWATERING WATER IS FOUND TO CONTAIN OIL, GREASE, ETC... IT MUST BE FILTERED OR PASSED THROUGH AN OIL/WATER SEPARATOR PRIOR TO DISCHARGE;
 - 1.3. WHEREVER POSSIBLE, DISCHARGE DEWATERING WATER TO VEGETATED UPLAND AREAS FOR INFILTRATION. WHERE THIS IS NOT POSSIBLE, DISCHARGE DEWATERING WATER INTO A FILTERING PIT CONSISTING OF A PERIMETER OF DOUBLE ROWS OF HAYBALES LINED WITH THREE LAYERS OF FILTER FABRIC. DO NOT DIRECT DEWATERING WATER INTO WETLANDS WITHOUT PRIOR TREATMENT;
 - 1.4. VELOCITY DISSIPATION MEASURES MUST BE INCLUDED AT ALL DISCHARGE POINTS (RIP-RAP OR CRUSHED STONE APRON).

INSPECTIONS

1. WHERE APPLICABLE, REFER TO SWPPP FOR INSPECTION & REPORTING REQUIREMENTS IN ADDITION TO THE FOLLOWING:
 - 1.1. THE SITE SHALL BE INSPECTED AT LEAST ONCE EVERY 14 CALENDAR DAYS AND WITHIN 24 HOURS OF THE OCCURRENCE OF A STORM EVENT OF 0.25 INCHES OR GREATER. INSPECTIONS MAY BE REDUCED DURING THE WINTER AND IN STABILIZED AREAS.
 - 1.2. ANY DEFICIENCIES SHALL BE REMEDIED IMMEDIATELY AND NO LATER THAN 7 DAYS AFTER DISCOVERY OF THE DEFICIENCY, AND IF POSSIBLE, PRIOR TO THE NEXT RAINFALL EVENT.



NOTE:

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTORS TO VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION. DIG SAFE IS TO BE NOTIFIED 72 WORKING HOURS IN ADVANCE OF CONSTRUCTION. DIG SAFE 1-888-DIG-SAFE

I CERTIFY THAT THIS PLAN HAS BEEN PREPARED IN CONFORMITY WITH THE RULES AND REGULATIONS OF THE REGISTERS OF DEEDS OF THE COMMONWEALTH OF MASSACHUSETTS

DATE: _____

APPROVAL UNDER SUBDIVISION CONTROL LAW IS REQUIRED.

DATE APPROVED: _____

MEDWAY PLANNING BOARD

DATE ENDORSED: _____

I CERTIFY THAT 20 DAYS HAVE ELAPSED SINCE PLANNING BOARD APPROVAL, AND NO APPEAL HAS BEEN FILED IN THIS OFFICE.

DATE: _____

MEDWAY TOWN CLERK

HILL VIEW ESTATES

DEFINITIVE SUBDIVISION
PERMANENT PRIVATE WAY

PLAN OF LAND
IN
MEDWAY, MA
MAR 3 PARCELS 33 & 33-01

ZONING ARI DISTRICT

OWNER 1/APPLICANT:
ONE NIRVANA DEVELOPMENT LLC
741 WASHINGTON STREET
CANTON, MA 02021

OWNER 2:
TWO NIRVANA WAY REAL ESTATE TRUST
2 NIRVANA WAY
MEDWAY, MA 02053

DATE: MARCH 12, 2012

COLONIAL ENGINEERING, INC.
11 AWL STREET
MEDWAY, MA 02053
508-533-1644

LEGACY ENGINEERING LLC
730 MAIN STREET (SUITE #2C)
MILLIS, MA 02054
508-376-8883

REVISION DATE: OCTOBER 28, 2013
COMMENTS FROM FEBRUARY 7, 2013
FROM PGC ASSOCIATES & TETRA TECH

REVISION DATE: MARCH 25, 2014
COMMENTS FROM CERTIFICATE OF ACTION

REVISION DATE: OCTOBER 10, 2017
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NEW STORMWATER REGULATIONS UPDATE

REVISION DATE: FEBRUARY 22, 2024
REVISIONS PER TOWN COMMENTS

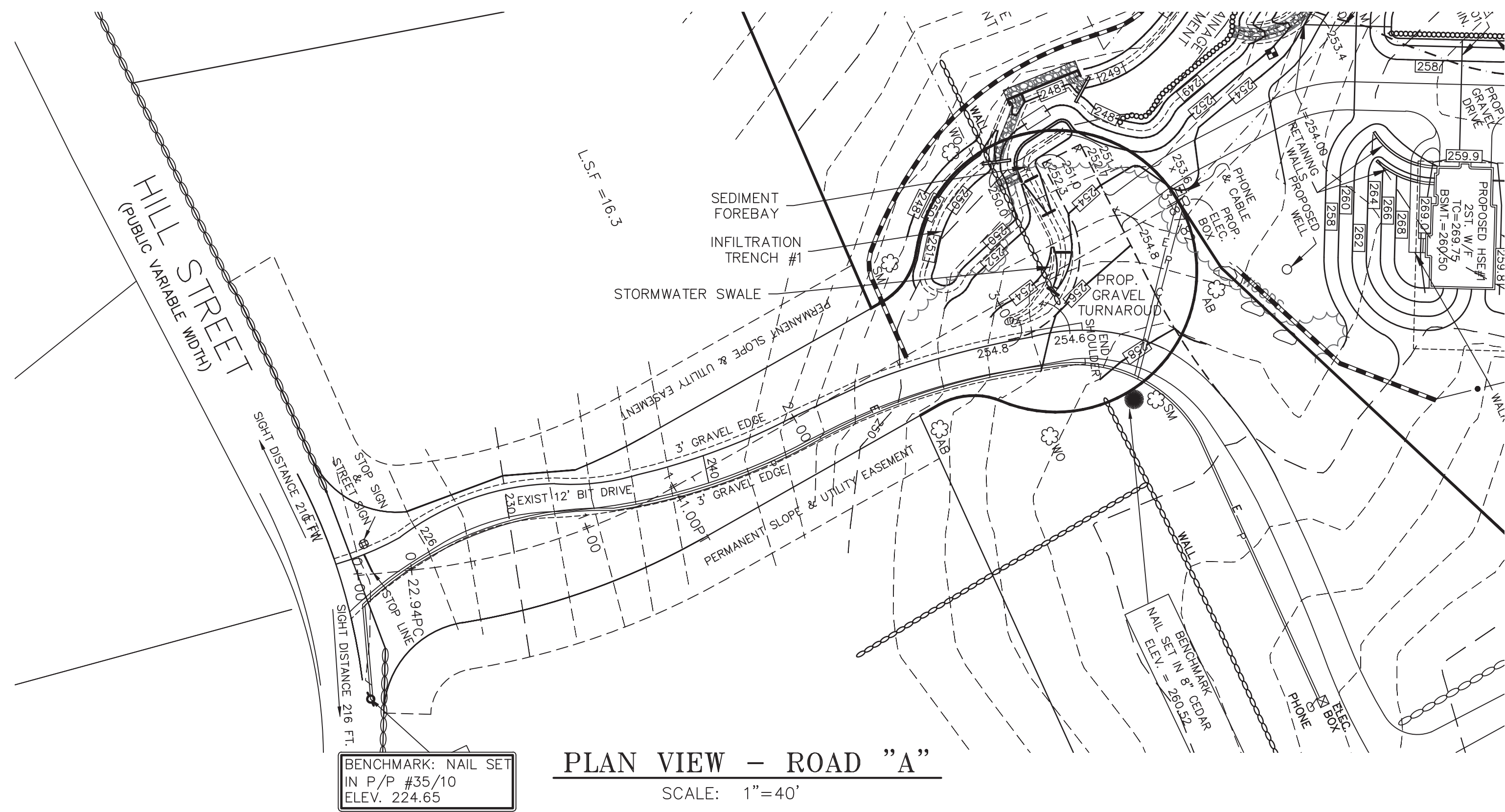
REVISION DATE: MARCH 8, 2024
REVISIONS PER TOWN COMMENTS



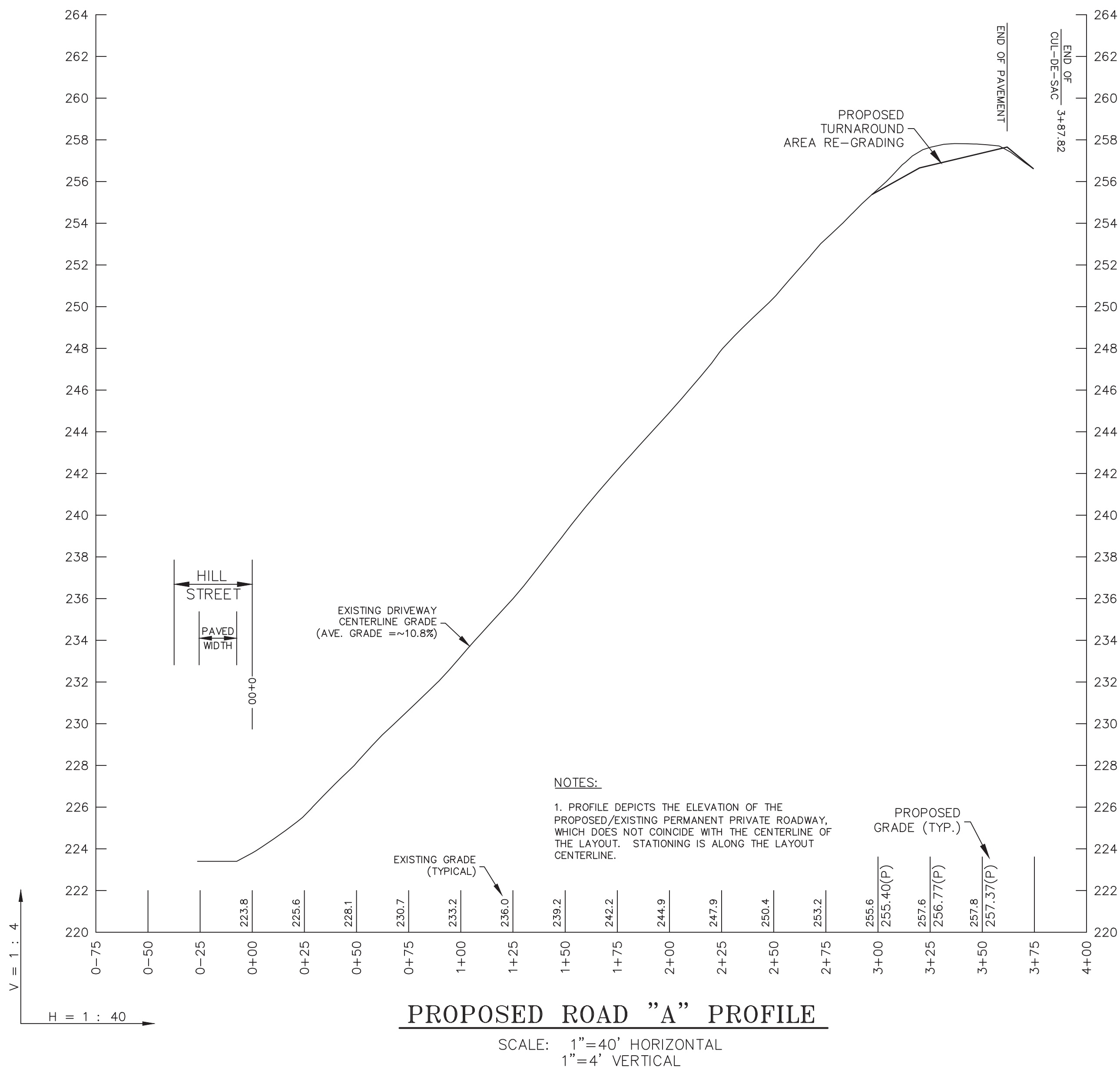
SCALE 1" = 40'

PROPOSED GRADING

SHEET 7 OF 10

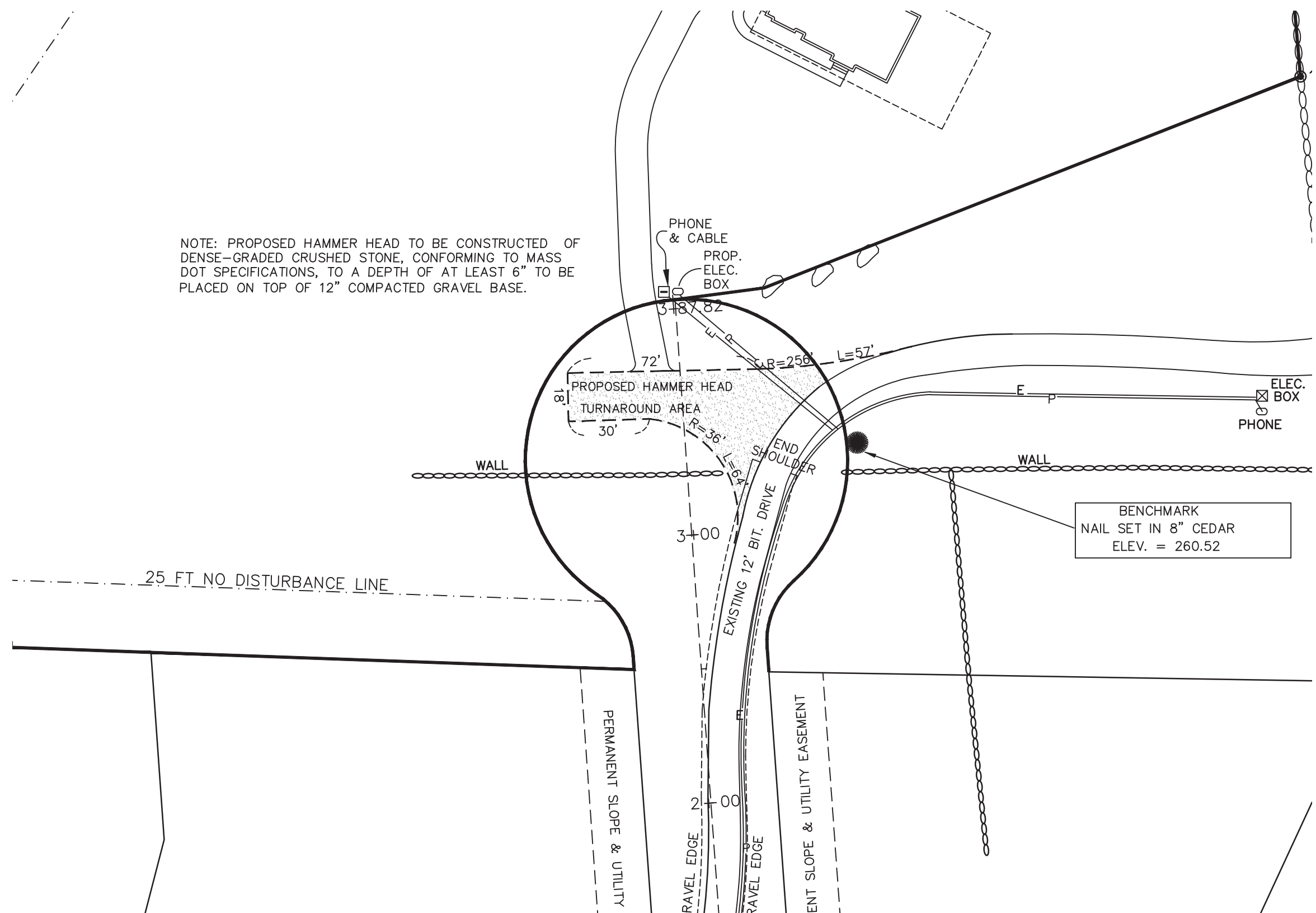


PLAN VIEW - ROAD "A"
SCALE: 1"=40'



PROPOSED ROAD "A" PROFILE
SCALE: 1"=40' HORIZONTAL
1"=4' VERTICAL

NAVD 1988 DATNUM



PLAN VIEW - HAMMER HEAD DETAIL
SCALE 1"=40'

NOTE:
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The present and future owners of Lots 10C & 10B are to provide a performance surety for the construction of ways and installation of municipal services in accordance with General Laws chapter 41 Section 81U.
The house address numbers shall be marked on a post at the end of each house lot driveway to facilitate easy identification from the private roadway for emergency and safety vehicles.

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REVISION DATE: MARCH 8, 2024
REVISIONS PER TOWN COMMENTS



Digitally signed by Daniel J. Merrikin, P.E.
Date: 2024.03.08 12:47:02 -05'00'

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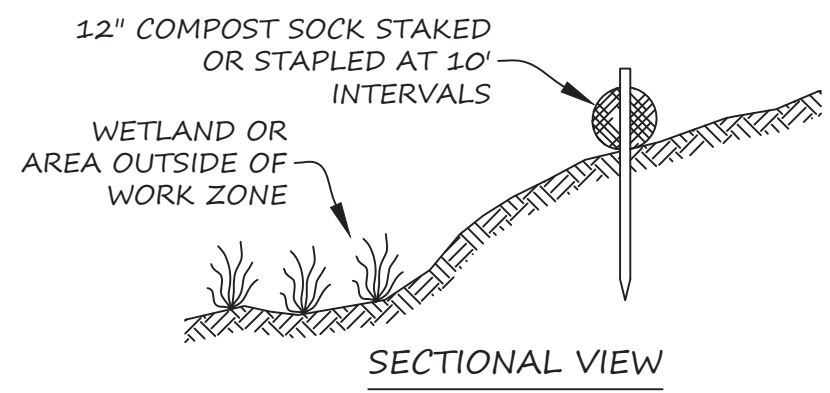
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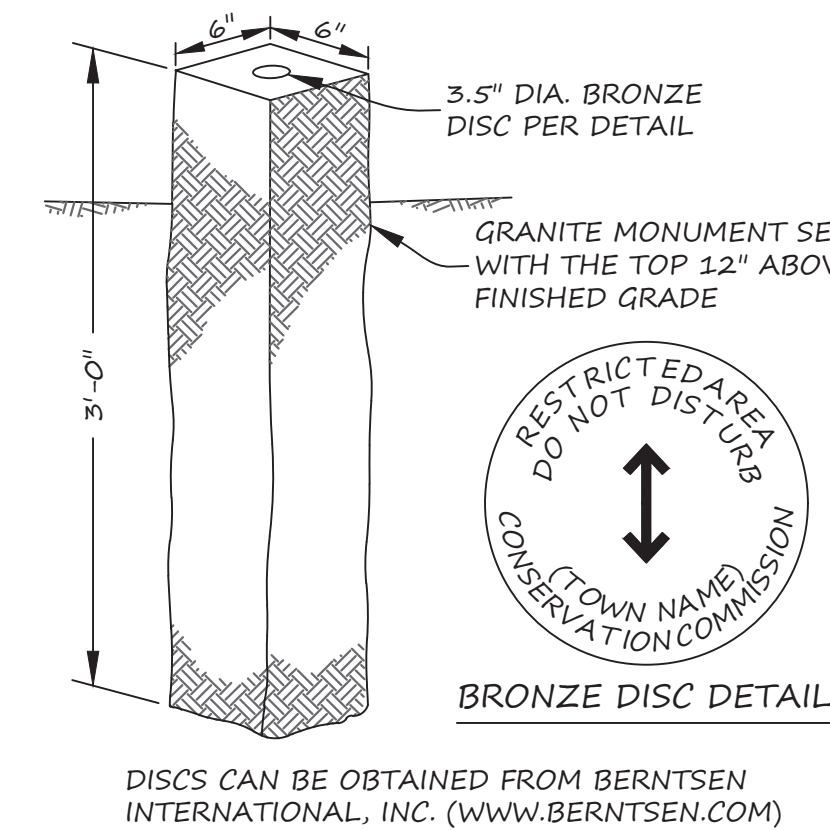
PLAN & PROFILE
SHEET 8 OF 10



NOTE: PHOTO-DEGRADABLE OR BIODEGRADABLE SOCK SHELL FILLED WITH COMPOST MEDIA. JOINTS SHALL BE OVERLAPPED BY AT LEAST TWO FEET. ORANGE SNOW FENCE IS ONLY NEEDED IF THERE IS EVIDENCE OF THE COMPOST SOCK BEING BURIED BY CONSTRUCTION ACTIVITIES.

EROSION CONTROL DETAIL (COMPOST SOCK)

NOT TO SCALE



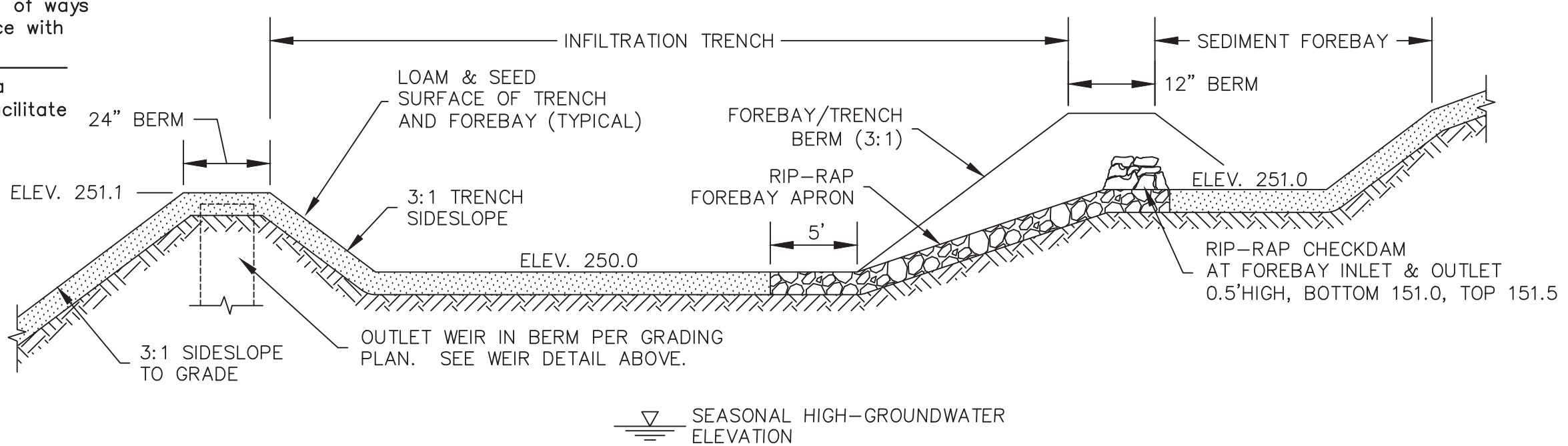
LIMIT OF WORK MONUMENT

NOT TO SCALE

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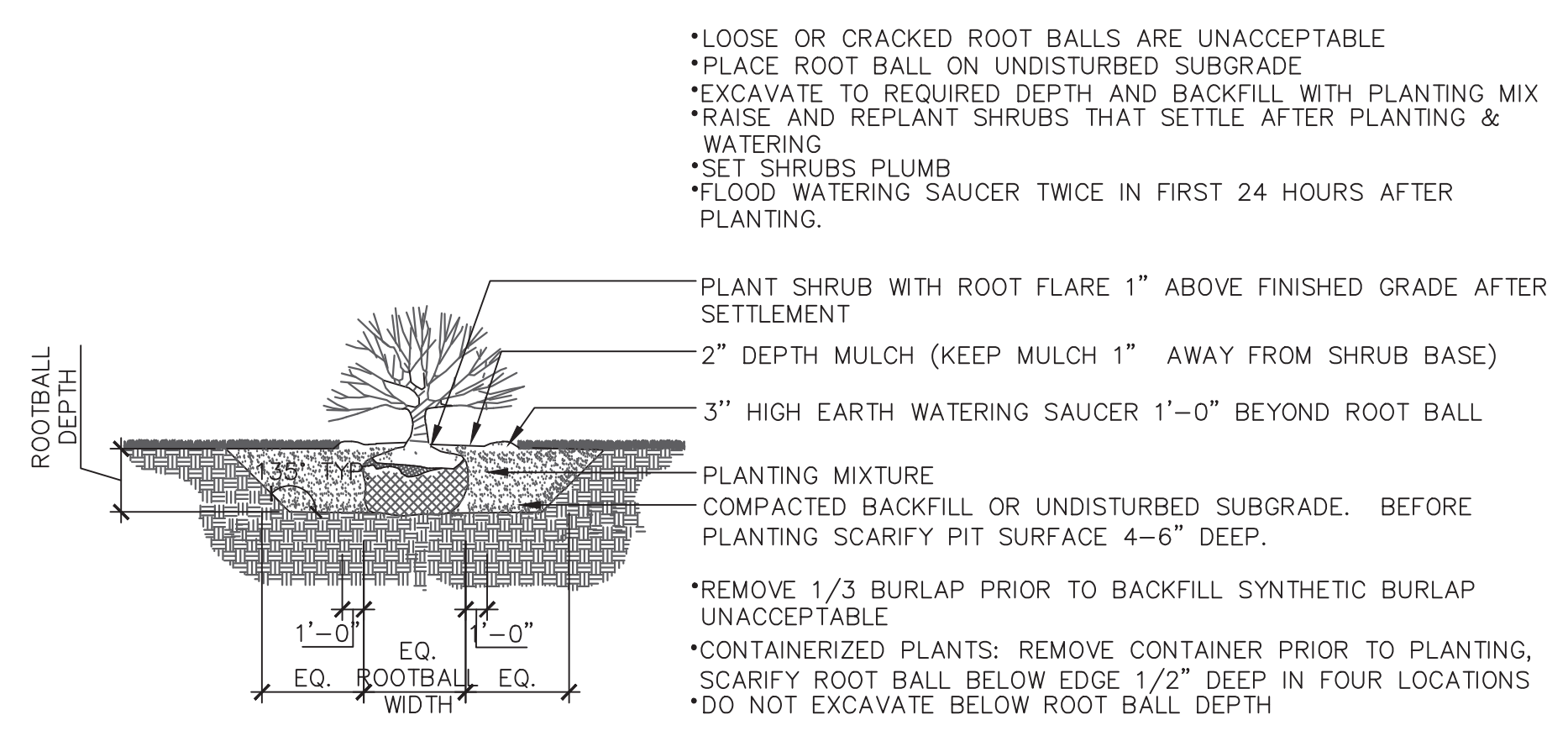


TRENCH CROSS-SECTION

- NOTES:
- SEE GRADING SHEETS AND PROFILE SHEETS FOR SIZE OF INLETS AND OUTLETS, OUTLET STRUCTURE SPECIFICATIONS, ELEVATION OF BOTTOM OF TRENCH, ELEVATION OF TOP OF TRENCH, SLOPE OF SIDESLOPES, AND OTHER DIMENSIONAL REQUIREMENTS FOR EACH TRENCH.
 - ALL TOPSOIL AND SUBSOIL BELOW THE BOTTOM OF THE BASIN SHALL BE REMOVED PRIOR TO TRENCH CONSTRUCTION. FILL MATERIALS UNDER THE TRENCH BOTTOM SHALL BE CLEAN GRAVEL. FILL FOR BERMS MAY BE SAND, LOAMY SAND OR SANDY LOAM. (TITLE V CLASSIFICATIONS).
 - BERM AREAS SHALL BE REMOVED OF ALL TOP AND SUB-SOIL AND OTHER UNSUITABLE SOILS. BERM CORE SHALL CONSIST OF ORDINARY BORROW INSTALLED IN 12-INCH LIFTS TO A MINIMUM 93% PROCTOR DENSITY.
 - BASIN SEEDING SHALL BE A NATIVE SEED MIX.

INFILTRATION TRENCH #1 DETAIL

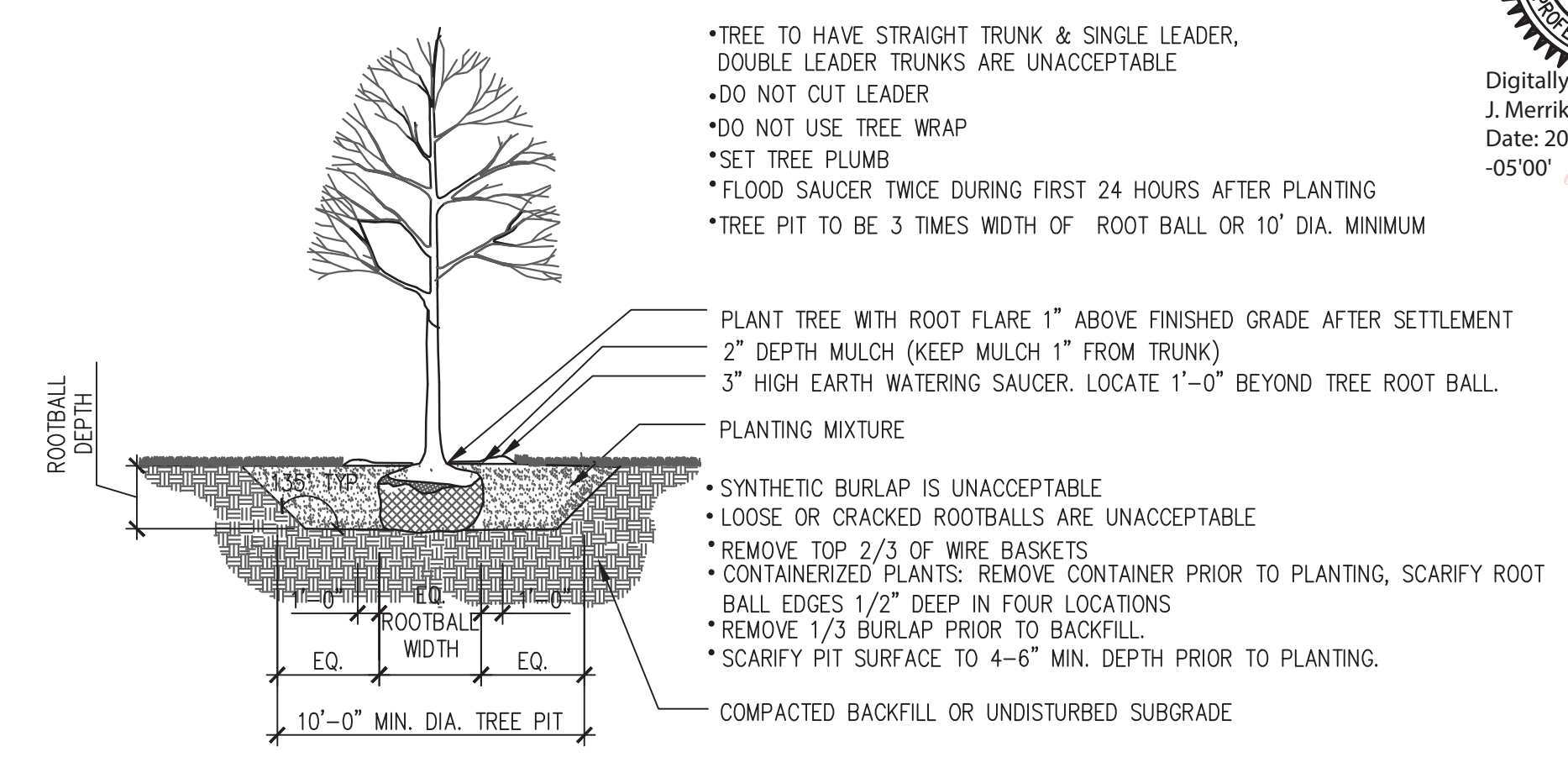
NOT TO SCALE



SHRUB PLANTING

Scale: NOT TO SCALE

- LOOSE OR CRACKED ROOT BALLS ARE UNACCEPTABLE
- PLACE ROOT BALL ON UNDISTURBED SUBGRADE
- EXCAVATE TO REQUIRED DEPTH AND BACKFILL WITH PLANTING MIX
- RAISE AND REPLANT SHRUBS THAT SETTLE AFTER PLANTING & WATERING
- SET SHRUBS PLUMB
- FLOOD WATERING SAUCER TWICE IN FIRST 24 HOURS AFTER PLANTING.
- PLANT SHRUB WITH ROOT FLARE 1" ABOVE FINISHED GRADE AFTER SETTLEMENT
- 2" DEPTH MULCH (KEEP MULCH 1" AWAY FROM SHRUB BASE)
- 3" HIGH EARTH WATERING SAUCER 1'-0" BEYOND ROOT BALL
- PLANTING MIXTURE
- COMPACTED BACKFILL OR UNDISTURBED SUBGRADE. BEFORE PLANTING SCARIFY PIT SURFACE 4-6" DEEP.
- REMOVE 1/3 BURLAP PRIOR TO BACKFILL SYNTHETIC BURLAP UNACCEPTABLE
- CONTAINERIZED PLANTS: REMOVE CONTAINER PRIOR TO PLANTING, SCARIFY ROOT BALL BELOW EDGE 1/2" DEEP IN FOUR LOCATIONS
- DO NOT EXCAVATE BELOW ROOT BALL DEPTH



DECIDUOUS TREE PLANTING

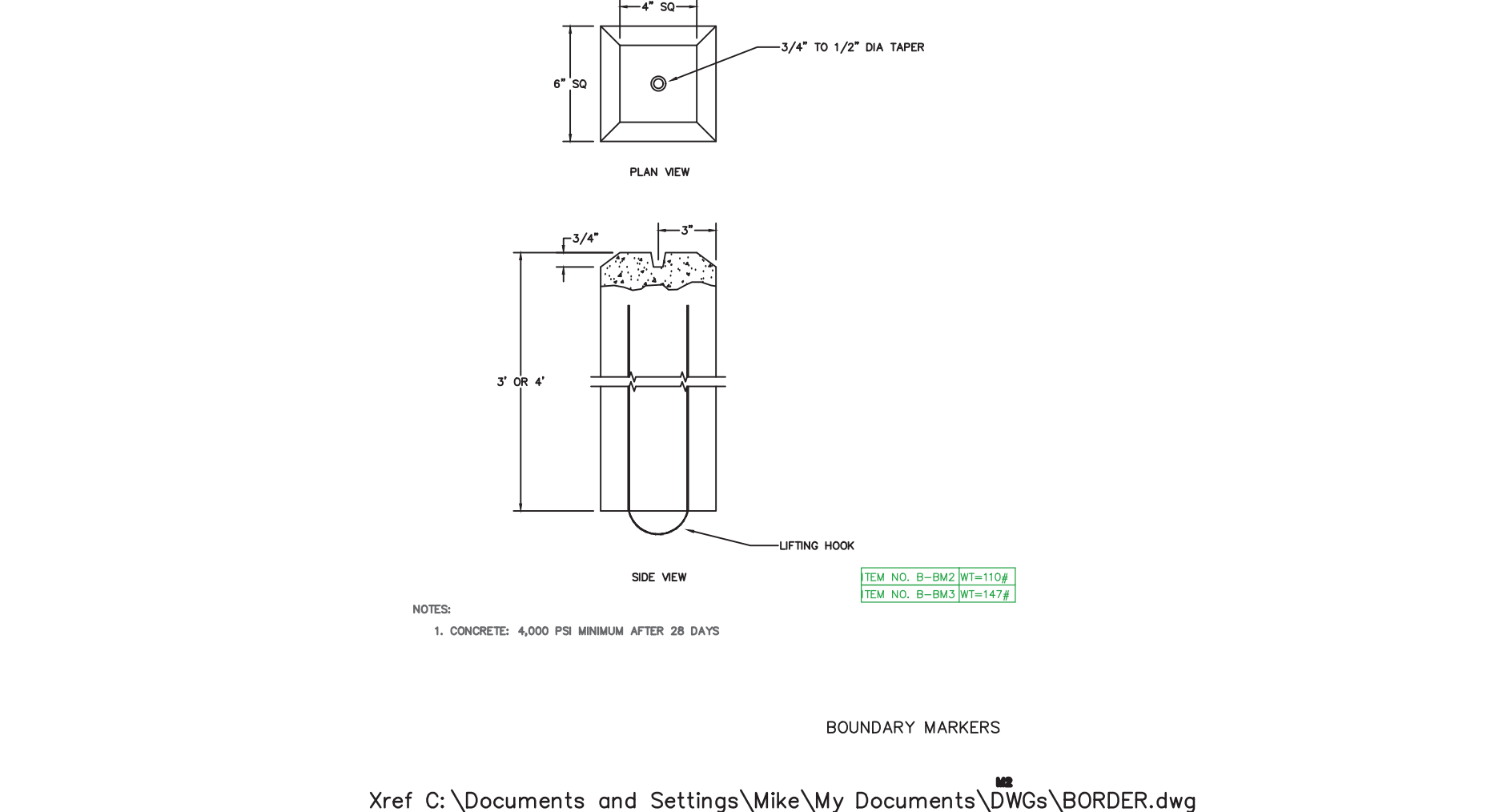
Scale: NOT TO SCALE

- TREE TO HAVE STRAIGHT TRUNK & SINGLE LEADER, DOUBLE LEADER TRUNKS ARE UNACCEPTABLE
- DO NOT CUT LEADER
- DO NOT USE TREE WRAP
- SET TREE PLUMB
- FLOOD SAUCER TWICE DURING FIRST 24 HOURS AFTER PLANTING
- TREE PIT TO BE 3 TIMES WIDTH OF ROOT BALL OR 10' DIA. MINIMUM
- PLANT TREE WITH ROOT FLARE 1" ABOVE FINISHED GRADE AFTER SETTLEMENT
- 2" DEPTH MULCH (KEEP MULCH 1" FROM TRUNK)
- 3" HIGH EARTH WATERING SAUCER. LOCATE 1'-0" BEYOND TREE ROOT BALL.
- PLANTING MIXTURE
- SYNTHETIC BURLAP IS UNACCEPTABLE
- LOOSE OR CRACKED ROOTBALLS ARE UNACCEPTABLE
- REMOVE TOP 2/3 OF WIRE BASKETS
- CONTAINERIZED PLANTS: REMOVE CONTAINER PRIOR TO PLANTING, SCARIFY ROOT BALL EDGES 1/2" DEEP IN FOUR LOCATIONS
- REMOVE 1/3 BURLAP PRIOR TO BACKFILL.
- SCARIFY PIT SURFACE TO 4-6" MIN. DEPTH PRIOR TO PLANTING.
- COMPACTED BACKFILL OR UNDISTURBED SUBGRADE

LOCATION OF TREE PLANTING AS SHOWN ON PLAN & PROFILE ALSO PROPOSED GRADING SHEETS



OUTLET WEIR HEADWALL INFILTRATION TRENCH #1



Xref C:\Documents and Settings\Mike\My Documents\DWGs\BORDER.dwg

NOTE: PRESENT AND FUTURE OWNERS ARE SUBJECT TO A PRIVATE ROAD MAINTENANCE AGREEMENT, RECORDED AT THE NORFOLK COUNTY REGISTRY OF DEEDS IN BOOK 36564, PAGE 548.

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DATE ENDORSED: _____

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DATE: _____

MEDWAY TOWN CLERK

NOTE:
THE SUBJECT PROPERTY IS NOT CLASSIFIED AS CHAPTER 61A OR 61B.

HILL VIEW ESTATES

DEFINITIVE SUBDIVISION
PERMANENT PRIVATE WAY

PLAN OF LAND
IN
MEDWAY, MA
MAP 3 PARCELS 33 & 33-01
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508-376-8883

DETAIL SHEET
SHEET 9 OF 10



Digitally signed by Daniel J. Merrikin, P.E.
Date: 2024.03.08 12:48:10 -05'00'

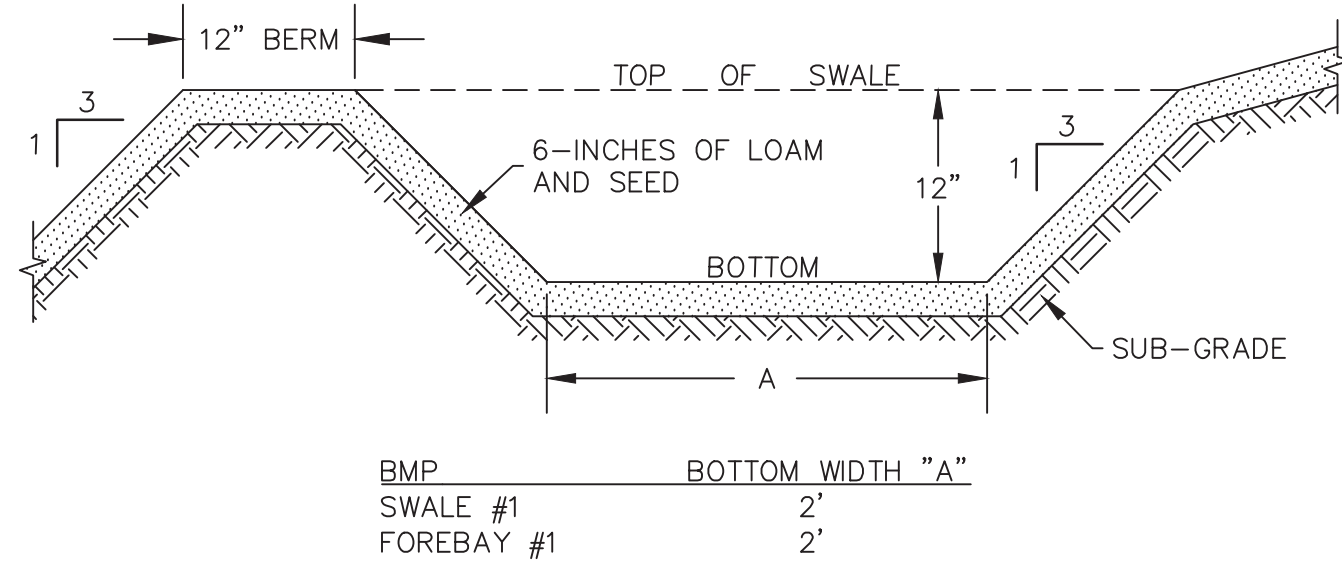
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NOTE:

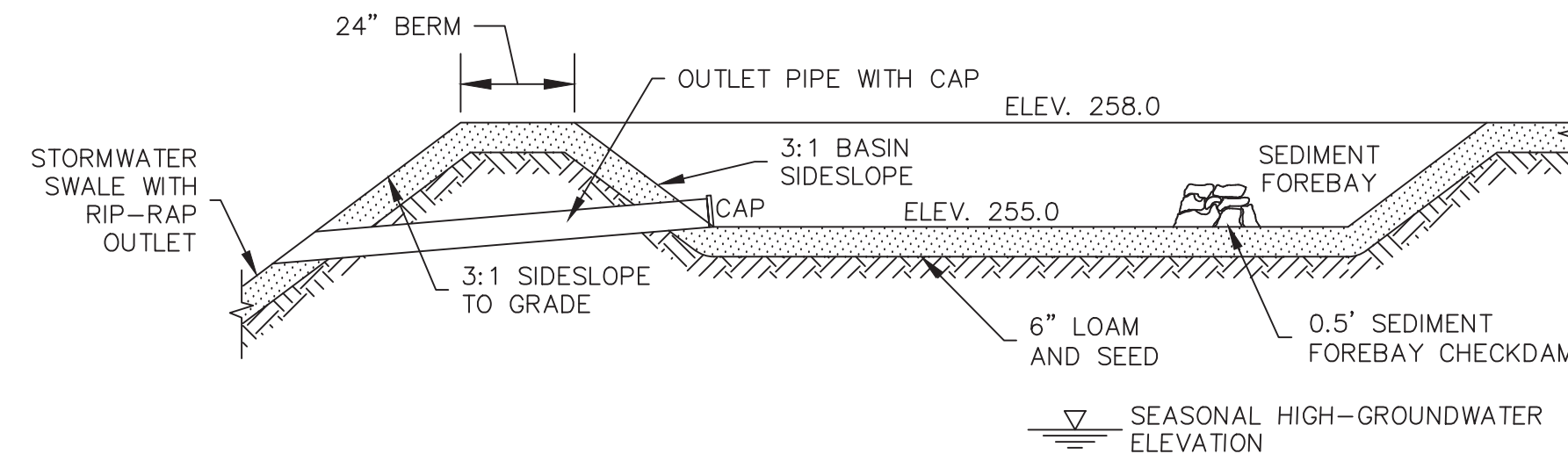
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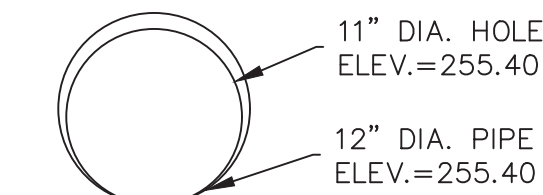
SEDIMENT FOREBAY #1 & STORMWATER SWALE DETAIL
NOT TO SCALE



TRENCH CROSS SECTION

NOTES:

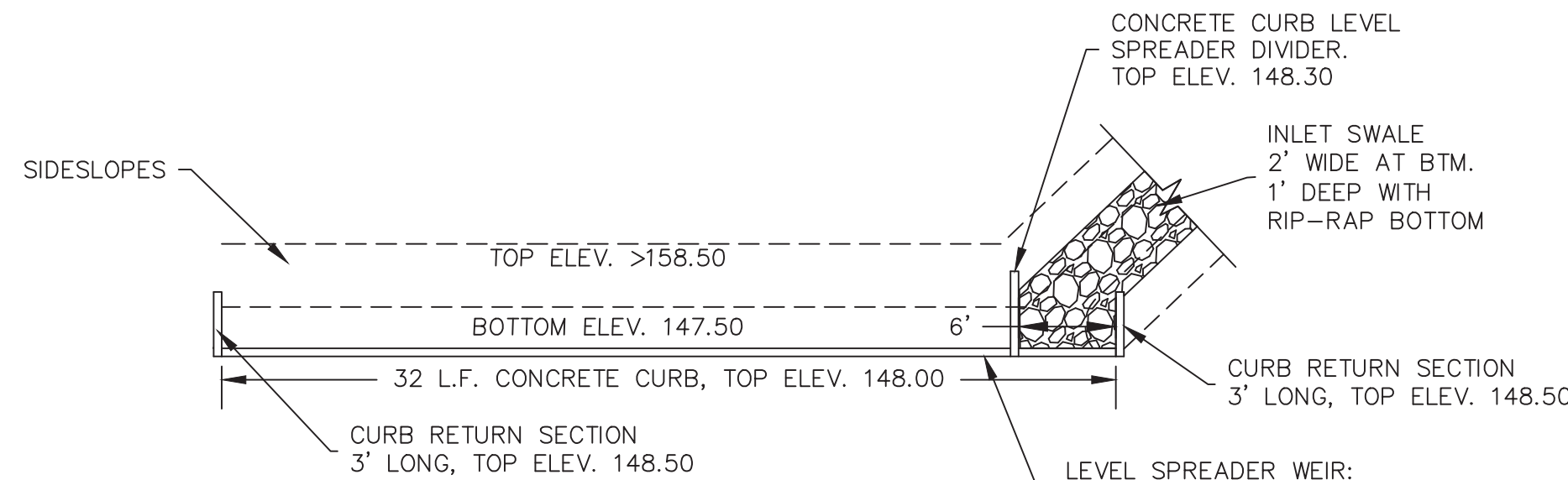
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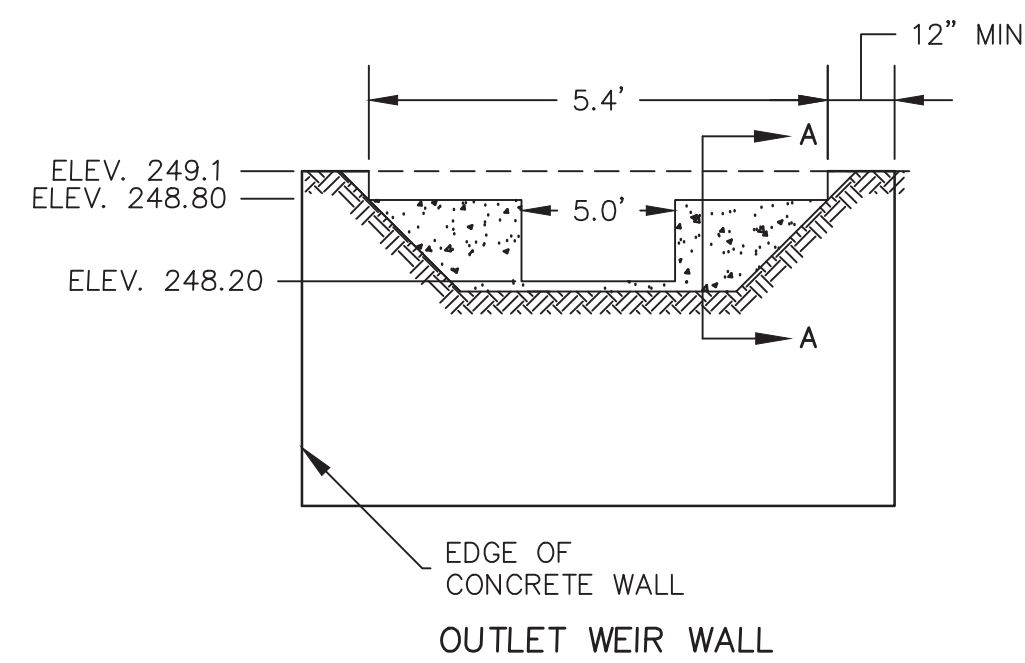
INFILTRATION BASIN OUTLET PIPE

NOTE: JOINT AND CAP ARE TO BE WATERTIGHT.

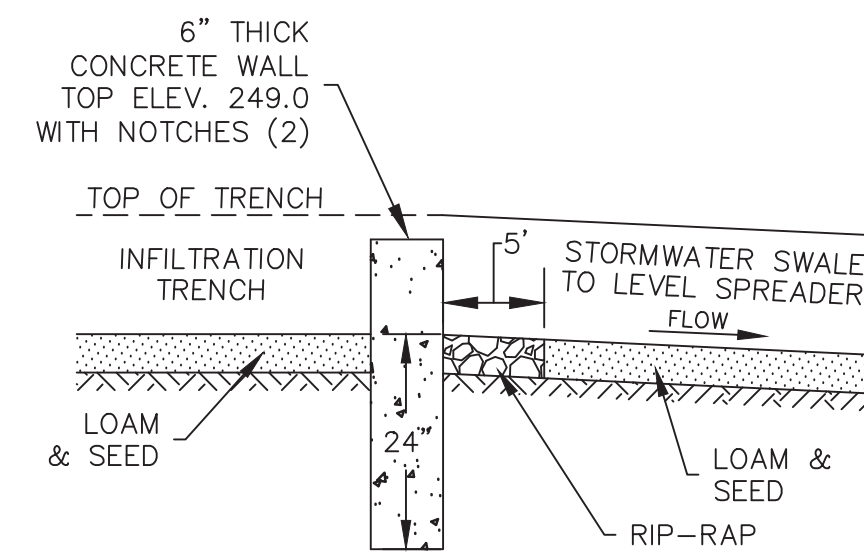
OUTLET PIPE CAP DETAILS



PLAN VIEW

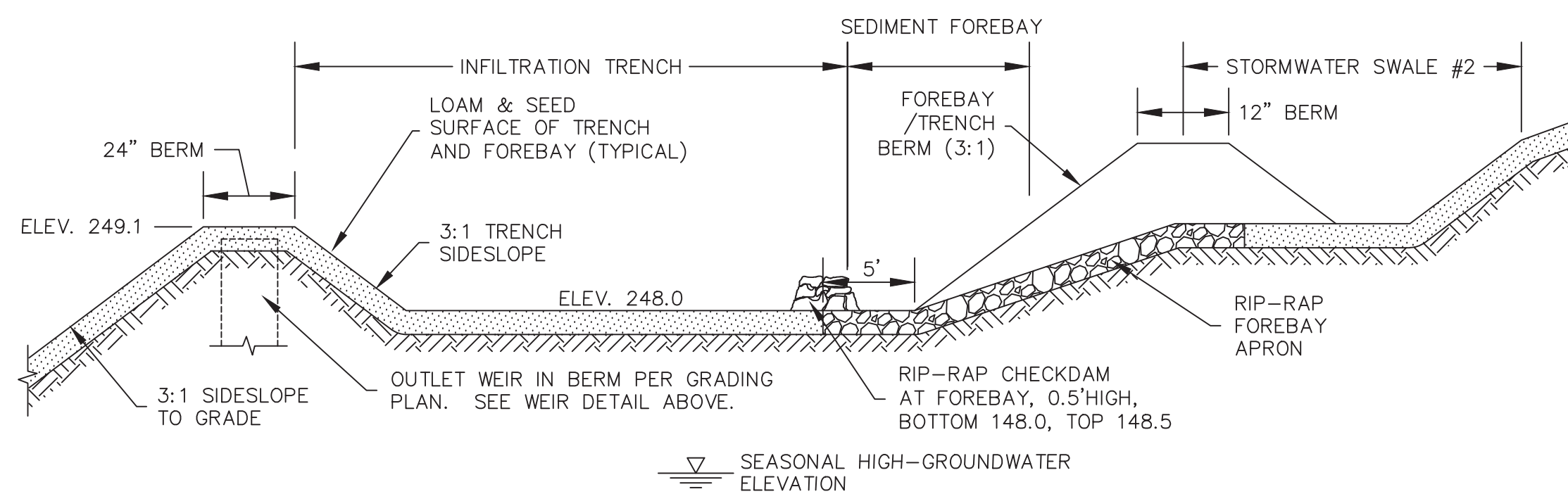


OUTLET WEIR WALL



SECTION A - THROUGH OUTLET WALL

OUTLET WEIR HEADWALL INFILTRATION TRENCH #2



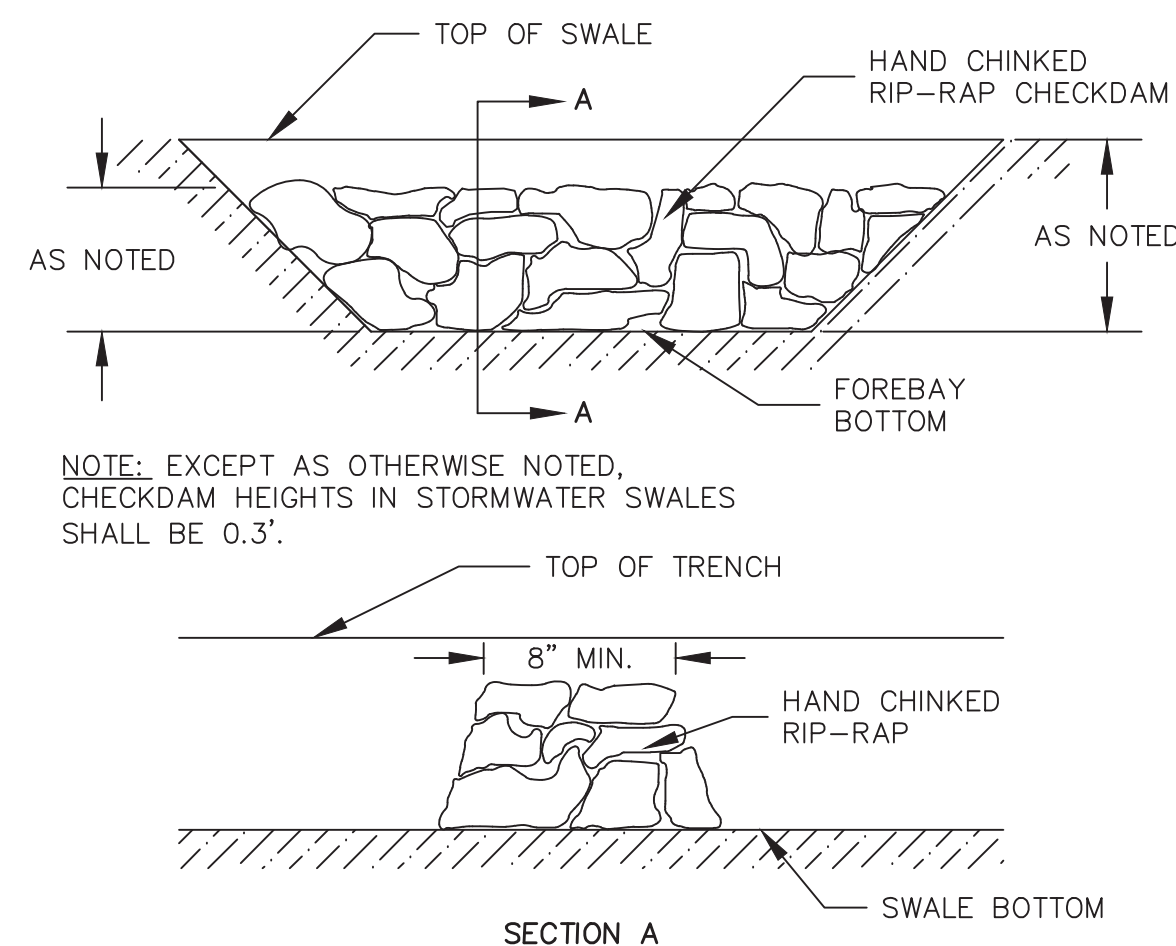
TRENCH CROSS-SECTION

NOTES:

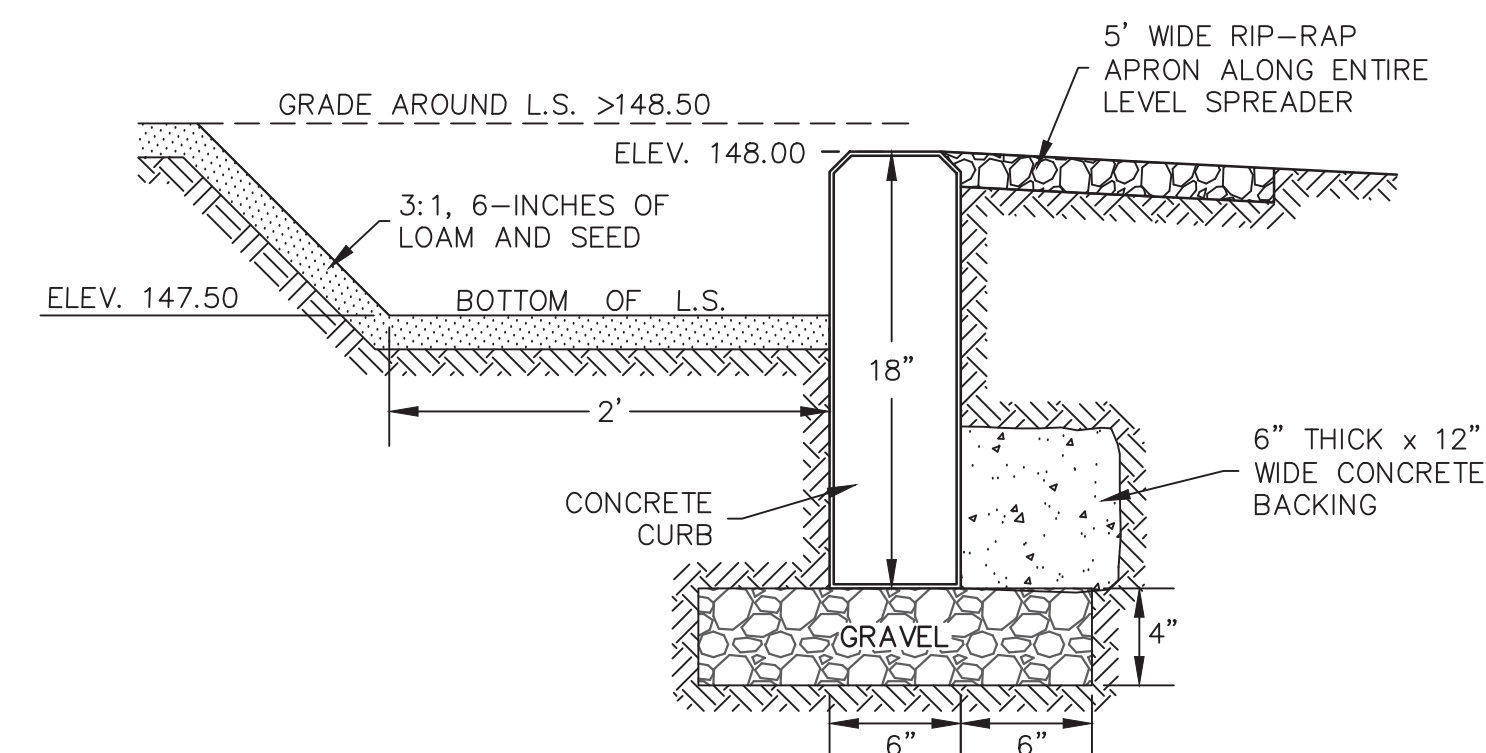
- SEE GRADING SHEETS AND PROFILE SHEETS FOR SIZE OF INLETS AND OUTLETS, OUTLET STRUCTURE SPECIFICATIONS, ELEVATION OF BOTTOM OF TRENCH, ELEVATION OF TOP OF TRENCH, SLOPE OF SIDESLOPES, AND OTHER DIMENSIONAL REQUIREMENTS FOR EACH TRENCH.
- ALL TOPSOIL AND SUBSOIL BELOW THE BOTTOM OF THE BASIN SHALL BE REMOVED PRIOR TO TRENCH CONSTRUCTION. FILL MATERIALS UNDER THE TRENCH BOTTOM SHALL BE CLEAN GRAVEL. FILL FOR BERMS MAY BE SAND, LOAMY SAND OR SANDY LOAM. (TITLE V CLASSIFICATIONS).
- BERM AREAS SHALL BE REMOVED OF ALL TOP AND SUB-SOIL AND OTHER UNSUITABLE SOILS. BERM CORE SHALL CONSIST OF ORDINARY BORROW INSTALLED IN 12-INCH LIFTS TO A MINIMUM 93% PROCTOR DENSITY.
- BASIN SEEDING SHALL BE A NATIVE SEED MIX.

INFILTRATION TRENCH #2 DETAIL

NOT TO SCALE



RIP-RAP CHECK DAM DETAIL
NOT TO SCALE



LEVEL SPREADER DETAIL
NOT TO SCALE

I CERTIFY THAT THIS PLAN HAS BEEN PREPARED IN CONFORMITY WITH THE RULES AND REGULATIONS OF THE REGISTERS OF DEEDS OF THE COMMONWEALTH OF MASSACHUSETTS

DATE: _____

APPROVAL UNDER SUBDIVISION CONTROL LAW IS REQUIRED.

DATE APPROVED: _____

MEDWAY PLANNING BOARD

DATE ENDORSED: _____

I CERTIFY THAT 20 DAYS HAVE ELAPSED SINCE PLANNING BOARD APPROVAL, AND NO APPEAL HAS BEEN FILED IN THIS OFFICE.

DATE: _____

MEDWAY TOWN CLERK

HILL VIEW ESTATES

DEFINITIVE SUBDIVISION
PERMANENT PRIVATE WAY

PLAN OF LAND
IN
MEDWAY, MA

MAP 3 PARCELS 33 & 33-01

ZONING ARI DISTRICT

OWNER 1/APPLICANT:
ONE NIRVANA DEVELOPMENT LLC
741 WASHINGTON STREET
CANTON, MA 02021

OWNER 2:
TWO NIRVANA WAY REAL ESTATE TRUST
2 NIRVANA WAY
MEDWAY, MA 02053

DATE: MARCH 12, 2012

COLONIAL ENGINEERING, INC.
11 AWL STREET
MEDWAY, MA 02053
508-533-1644

LEGACY ENGINEERING LLC
730 MAIN STREET (SUITE #2C)
MILLIS, MA 02054
508-376-8883

REVISION DATE: OCTOBER 28, 2013
COMMENTS FROM FEBRUARY 7, 2013
FROM PGC ASSOCIATES & TETRA TECH

REVISION DATE: MARCH 25, 2014
COMMENTS FROM CERTIFICATE OF ACTION

REVISION DATE: OCTOBER 10, 2017

REVISION DATE: JANUARY 9, 2024
NEW STORMWATER REGULATIONS UPDATE

REVISION DATE: FEBRUARY 22, 2024
REVISIONS PER TOWN COMMENTS

REVISION DATE: MARCH 8, 2024
REVISIONS PER TOWN COMMENTS

DETAIL SHEET

SHEET 10 OF 10